

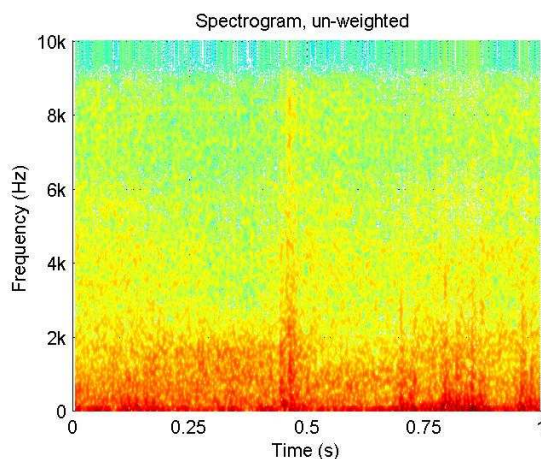
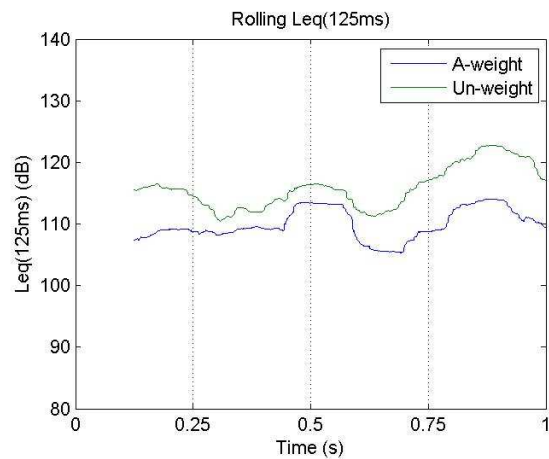
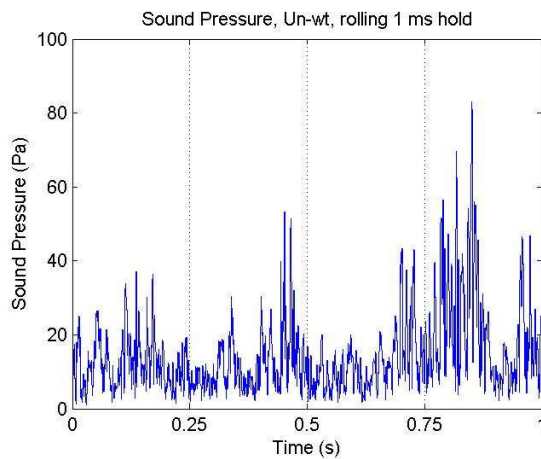
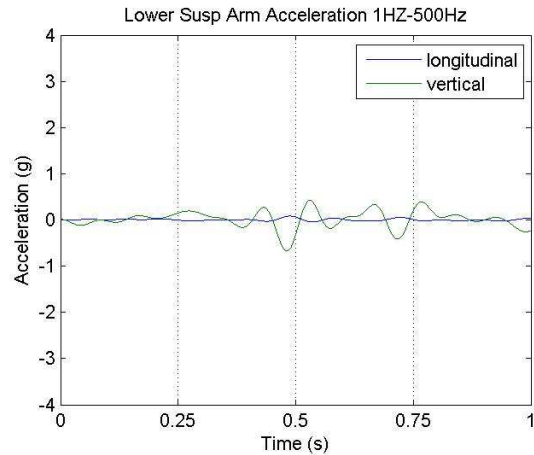
Bridge Structure Number: BSN82

Name: GREAT NORTH RD BRIDGE No.1
Route Position: 16 0 8.22
Direction: Increasing
Joint Type: Rubber extrusion unreinforced
Road Surface Type: Asphalt

Test Details:

Operator: I.Kvatch
Date: 15-05-13
Time: 11-10-54
Marker No: 1

Latitude: -36.871650
Longitude: 174.708712
Speed (km/h): 79.1



Acceleration – Maximum vertical (g): 0.67
Acceleration – Maximum longitudinal (g): 0.08
Displacement – Maximum vertical (mm): 6.81
Displacement – Maximum longitudinal (mm): 0.85
Noise Peak Sound Pressure (Pa): 83.08
Average sound level, Leq(1s) (dB): 117.25
Average sound level, LAeq(1s) (dB): 110.48
Maximum sound level, Leq(125ms) (dB): 122.75
Maximum sound level, LAeq(125ms) (dBA): 114.05

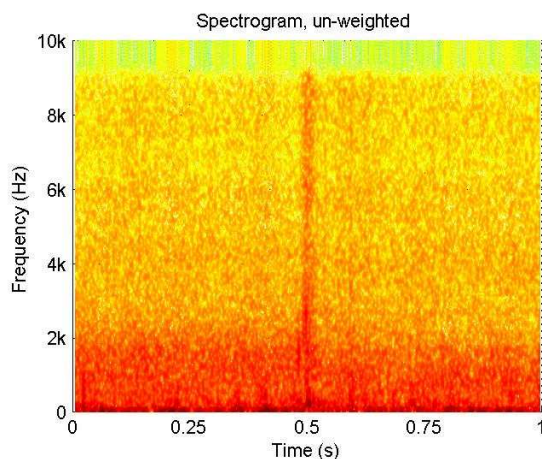
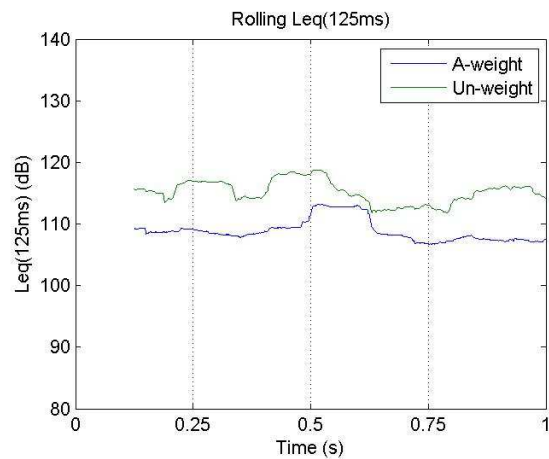
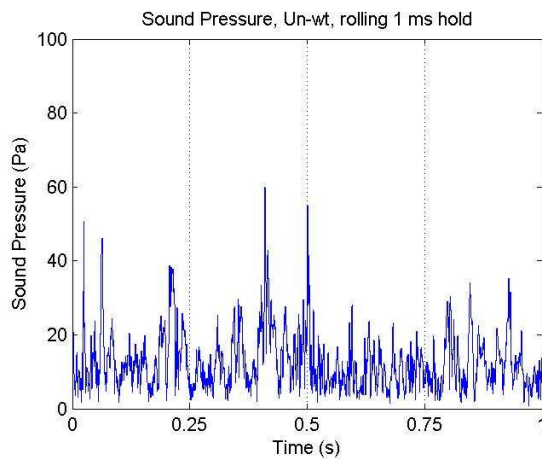
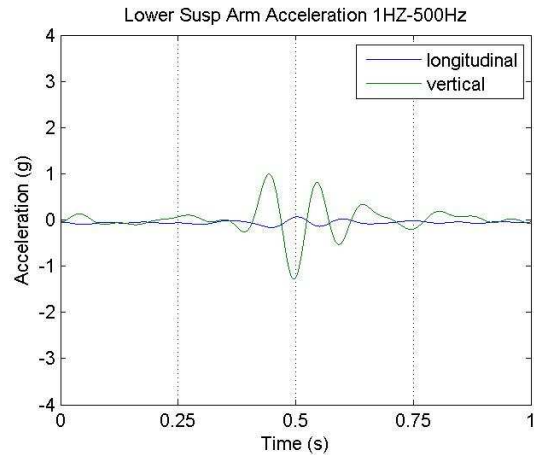
Bridge Structure Number: BSN82

Name: GREAT NORTH RD BRIDGE No.1
Route Position: 16 0 8.22
Direction: Increasing
Joint Type: Rubber extrusion unreinforced
Road Surface Type: Asphalt

Test Details:

Operator: I.Kvatch
Date: 15-05-13
Time: 11:28:59
Marker No: 1

Latitude: -36.872185
Longitude: 174.705627
Speed (km/h): 80.4



Acceleration – Maximum vertical (g): 1.28
Acceleration – Maximum longitudinal (g): 0.16
Displacement – Maximum vertical (mm): 12.98
Displacement – Maximum longitudinal (mm): 1.65
Noise Peak Sound Pressure (Pa): 59.92
Average sound level, Leq(1s) (dB): 115.50
Average sound level, LAeq(1s) (dB): 109.27
Maximum sound level, Leq(125ms) (dB): 118.76
Maximum sound level, LAeq(125ms) (dBA): 113.16

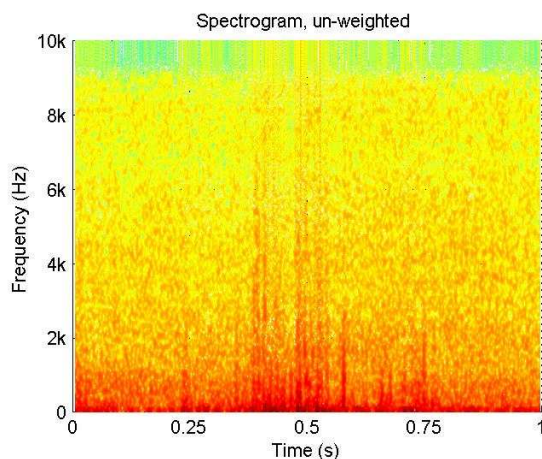
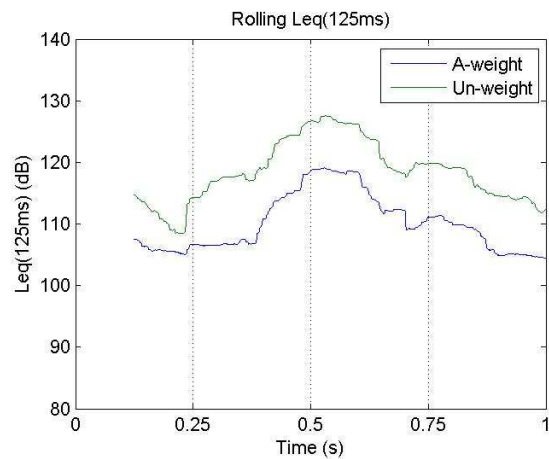
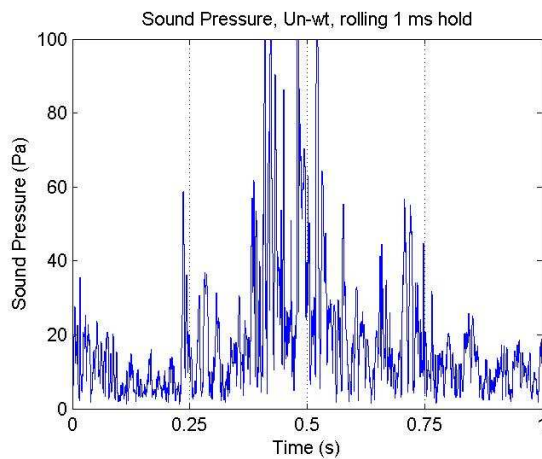
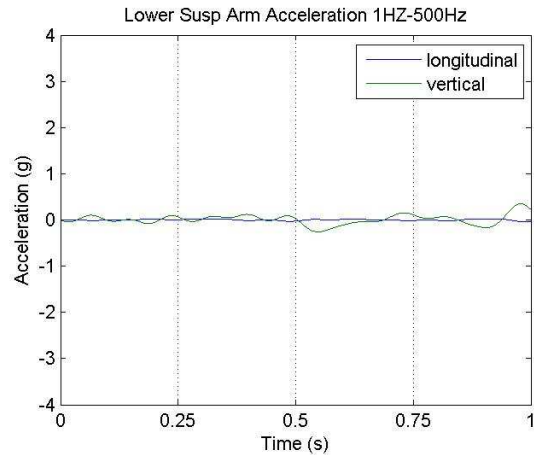
Bridge Structure Number: BSN82

Name: GREAT NORTH RD BRIDGE No.1
Route Position: 16 0 8.22
Direction: Increasing
Joint Type: Rubber extrusion unreinforced
Road Surface Type: Asphalt

Test Details:

Operator: I.Kvatch
Date: 15-05-13
Time: 11-10-54
Marker No: 2

Latitude: -36.871677
Longitude: 174.708218
Speed (km/h): 79.1



Acceleration – Maximum vertical (g): 0.35
Acceleration – Maximum longitudinal (g): 0.04
Displacement – Maximum vertical (mm): 3.56
Displacement – Maximum longitudinal (mm): 0.45
Noise Peak Sound Pressure (Pa): 168.39
Average sound level, Leq(1s) (dB): 120.79
Average sound level, LAeq(1s) (dB): 112.37
Maximum sound level, Leq(125ms) (dB): 127.60
Maximum sound level, LAeq(125ms) (dBA): 119.10

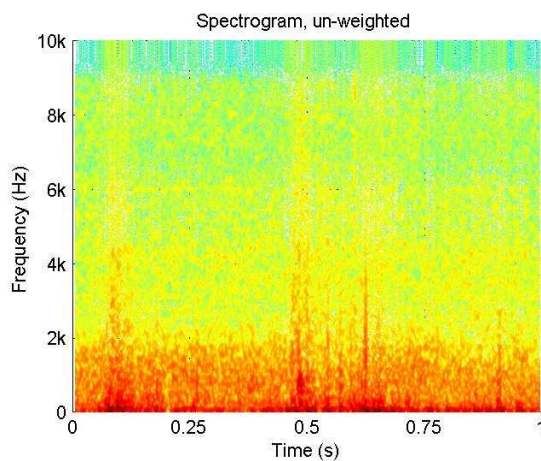
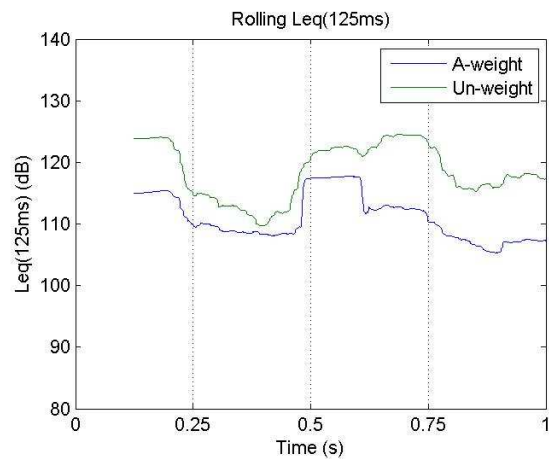
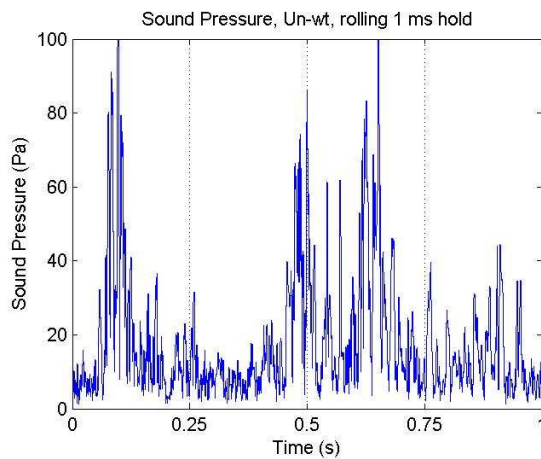
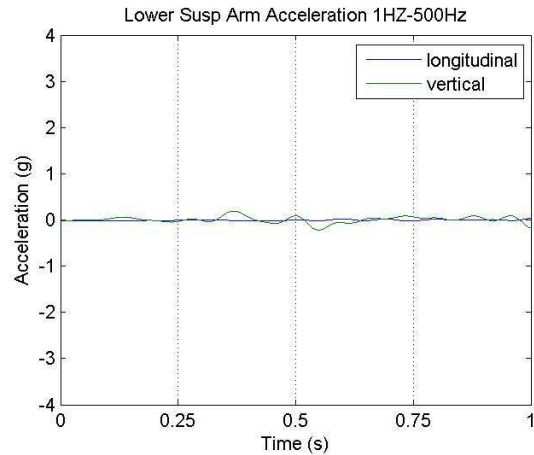
Bridge Structure Number: BSN82

Name: GREAT NORTH RD BRIDGE No.1
Route Position: 16 0 8.22
Direction: Increasing
Joint Type: Rubber extrusion unreinforced
Road Surface Type: Asphalt

Test Details:

Operator: I.Kvatch
Date: 15-05-13
Time: 11:28:59
Marker No: 2

Latitude: -36.872345
Longitude: 174.704905
Speed (km/h): 80.2



Acceleration – Maximum vertical (g): 0.22
Acceleration – Maximum longitudinal (g): 0.04
Displacement – Maximum vertical (mm): 2.21
Displacement – Maximum longitudinal (mm): 0.39
Noise Peak Sound Pressure (Pa): 142.45
Average sound level, Leq(1s) (dB): 120.36
Average sound level, LAeq(1s) (dB): 112.48
Maximum sound level, Leq(125ms) (dB): 124.57
Maximum sound level, LAeq(125ms) (dBA): 117.72

Bridge Structure Number: BSN83

Name: GREAT NORTH RD BRIDGE No. 2

Route Position: 16 0 8.22

Direction: Decreasing

Joint Type: Rubber extrusion unreinforced

Road Surface Type: Asphalt

Test Details:

Operator: I.Kvatch

Date: 15-05-13

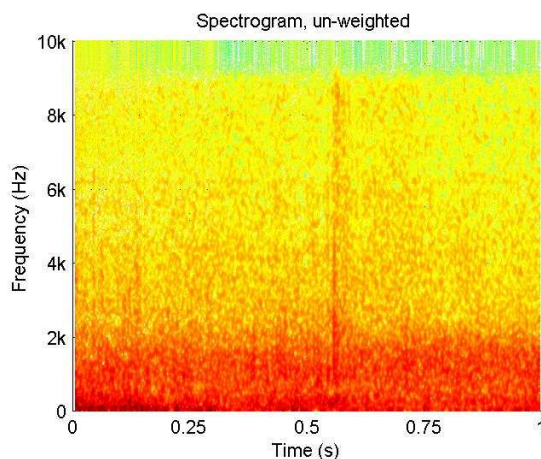
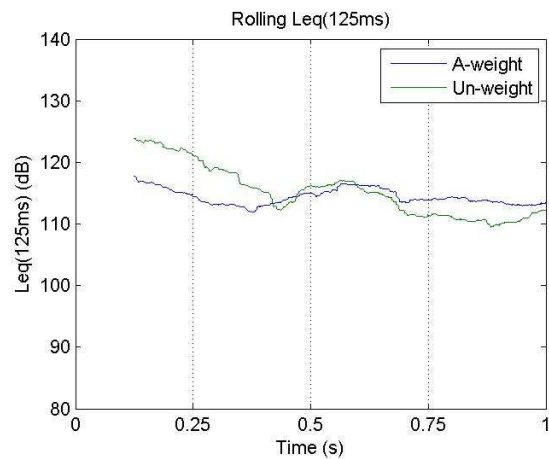
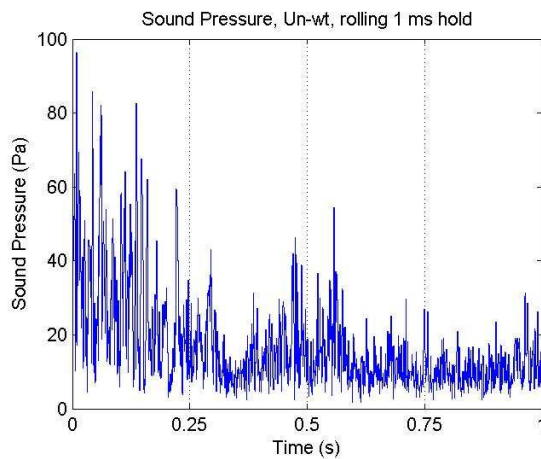
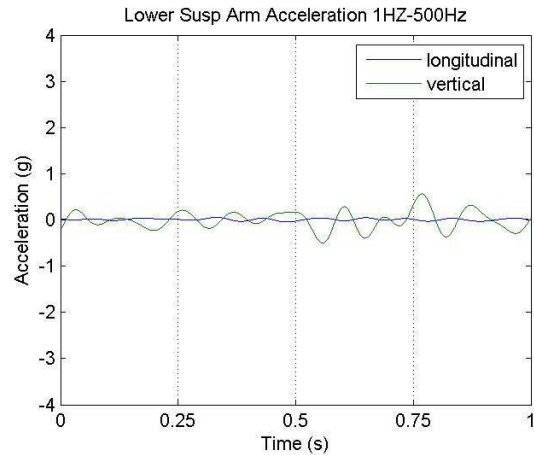
Time: 11:24:08

Marker No: 1

Latitude: -36.872273

Longitude: 174.704613

Speed (km/h): 78.5



Acceleration – Maximum vertical (g): 0.56
Acceleration – Maximum longitudinal (g): 0.06
Displacement – Maximum vertical (mm): 5.72
Displacement – Maximum longitudinal (mm): 0.56
Noise Peak Sound Pressure (Pa): 96.33
Average sound level, Leq(1s) (dB): 118.17
Average sound level, LAeq(1s) (dB): 114.89
Maximum sound level, Leq(125ms) (dB): 123.82
Maximum sound level, LAeq(125ms) (dBA): 117.74

Bridge Structure Number: BSN83

Name: GREAT NORTH RD BRIDGE No. 2

Route Position: 16 0 8.22

Direction: Decreasing

Joint Type: Rubber extrusion unreinforced

Road Surface Type: Asphalt

Test Details:

Operator: I.Kvatch

Date: 15-05-13

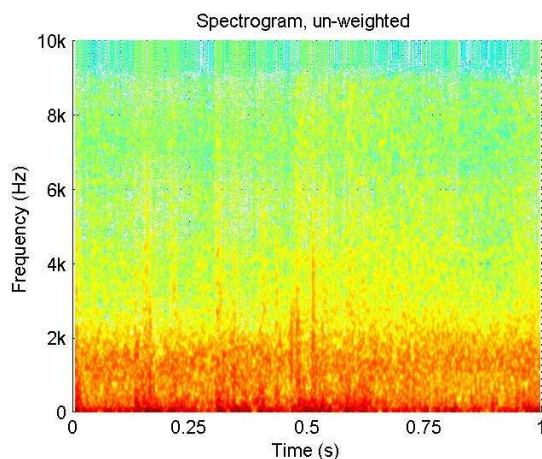
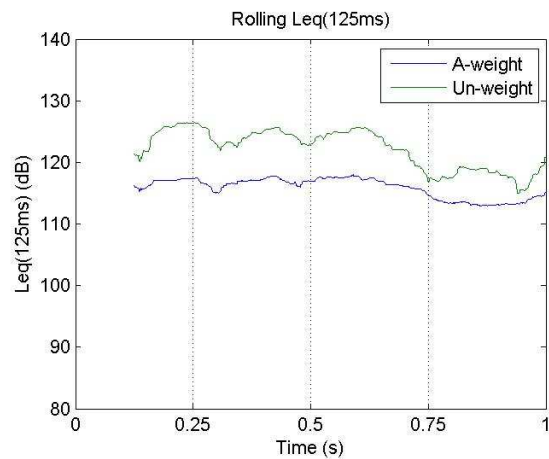
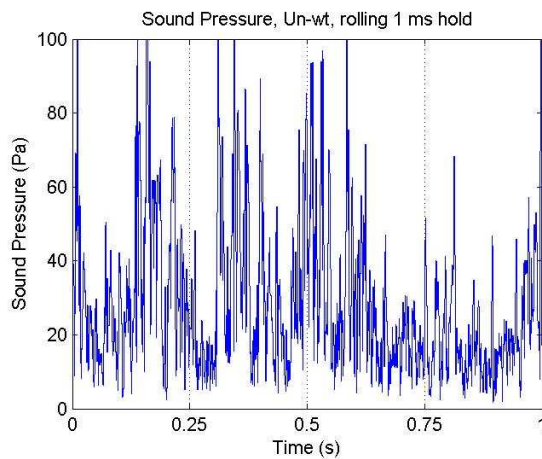
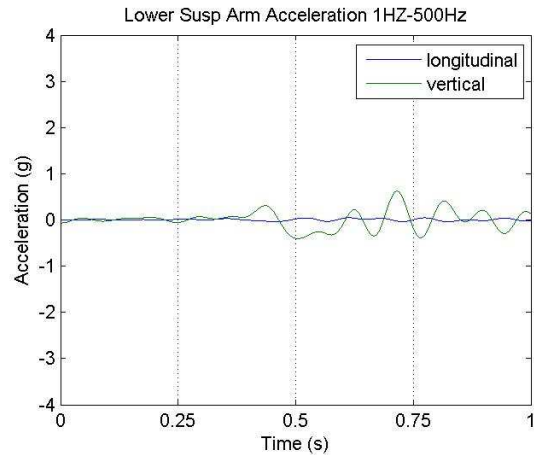
Time: 11:24:08

Marker No: 2

Latitude: -36.872188

Longitude: 174.705090

Speed (km/h): 78.5



Acceleration – Maximum vertical (g): 0.62
Acceleration – Maximum longitudinal (g): 0.05
Displacement – Maximum vertical (mm): 6.32
Displacement – Maximum longitudinal (mm): 0.50
Noise Peak Sound Pressure (Pa): 154.20
Average sound level, Leq(1s) (dB): 123.14
Average sound level, LAeq(1s) (dB): 116.21
Maximum sound level, Leq(125ms) (dB): 126.42
Maximum sound level, LAeq(125ms) (dBA): 117.97

Bridge Structure Number: BSN84

Name: ROSEBANK BRIDGE NO1

Route Position: 16 7 0

Direction: Decreasing

Joint Type: Rubber extrusion unreinforced

Road Surface Type: Asphalt

Test Details:

Operator: I.Kvatch

Date: 15-05-13

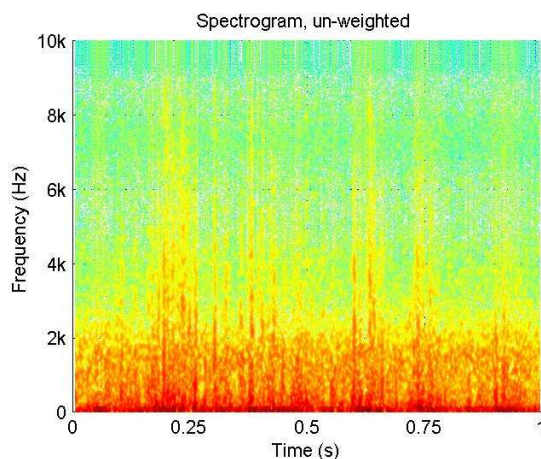
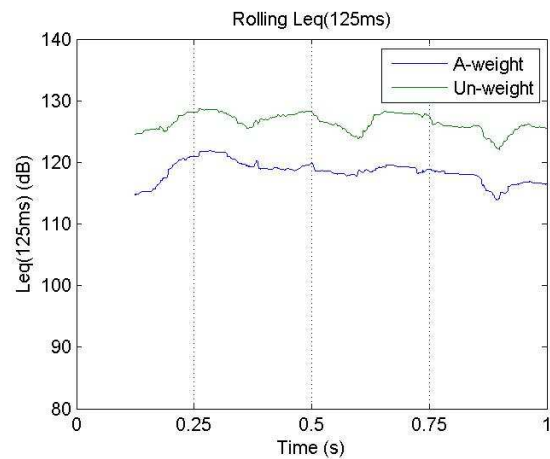
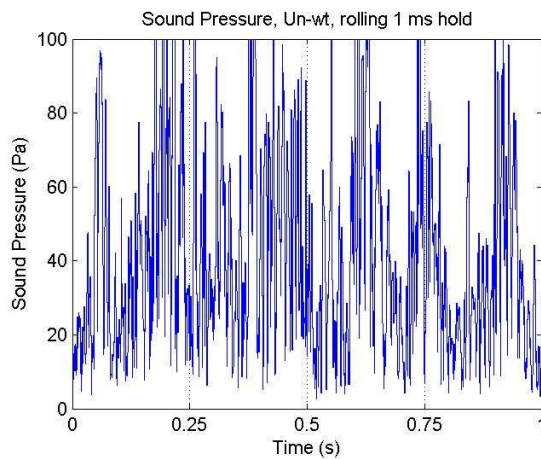
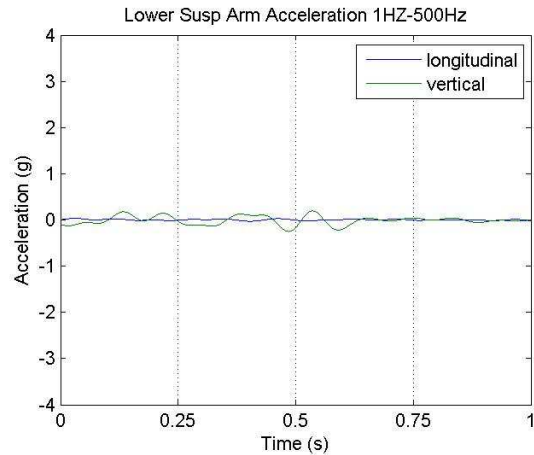
Time: 11-22-34

Marker No: 1

Latitude: -36.871935

Longitude: 174.684867

Speed (km/h): 79.1



Acceleration – Maximum vertical (g): 0.25
Acceleration – Maximum longitudinal (g): 0.03
Displacement – Maximum vertical (mm): 2.53
Displacement – Maximum longitudinal (mm): 0.34
Noise Peak Sound Pressure (Pa): 165.17
Average sound level, Leq(1s) (dB): 126.48
Average sound level, LAeq(1s) (dB): 118.45
Maximum sound level, Leq(125ms) (dB): 128.69
Maximum sound level, LAeq(125ms) (dBA): 121.92

Bridge Structure Number: BSN84

Name: ROSEBANK BRIDGE NO1

Route Position: 16 7 0

Direction: Decreasing

Joint Type: Rubber extrusion unreinforced

Road Surface Type: Asphalt

Test Details:

Operator: I.Kvatch

Date: 15-05-13

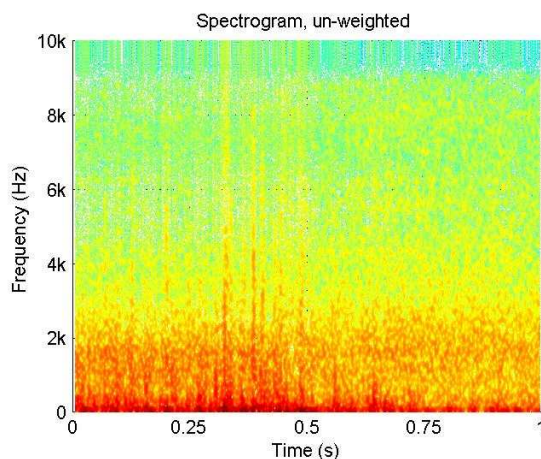
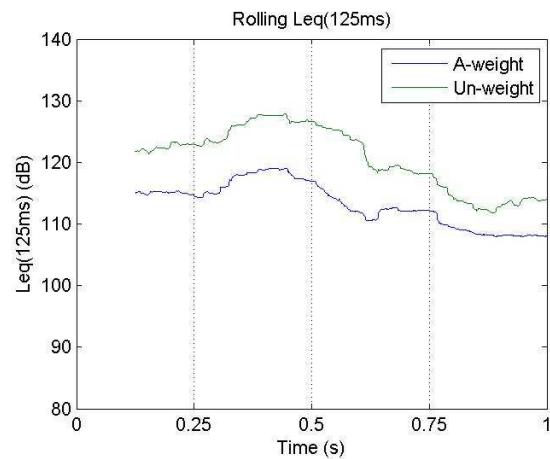
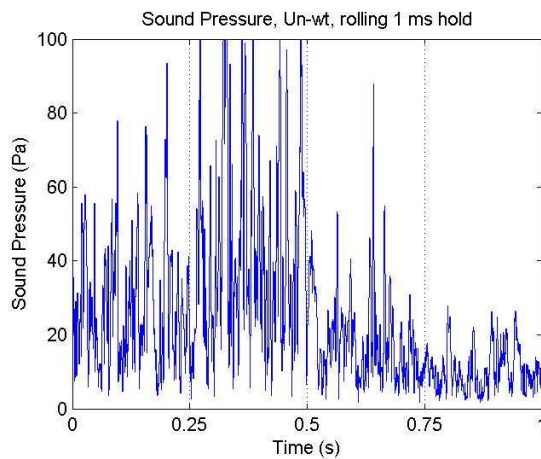
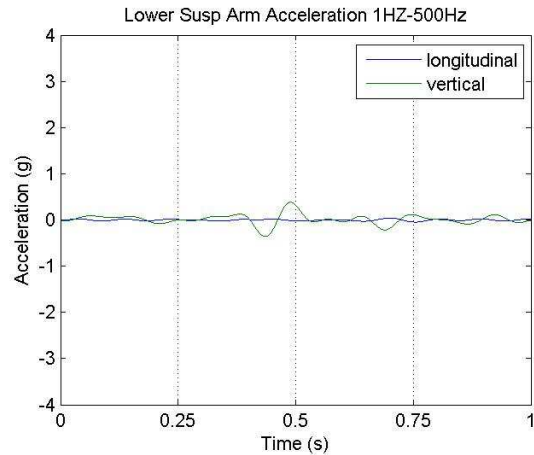
Time: 11-22-34

Marker No: 2

Latitude: -36.871932

Longitude: 174.686083

Speed (km/h): 78.3



Acceleration – Maximum vertical (g): 0.38
Acceleration – Maximum longitudinal (g): 0.05
Displacement – Maximum vertical (mm): 3.90
Displacement – Maximum longitudinal (mm): 0.48
Noise Peak Sound Pressure (Pa): 156.68
Average sound level, Leq(1s) (dB): 122.78
Average sound level, LAeq(1s) (dB): 114.41
Maximum sound level, Leq(125ms) (dB): 127.95
Maximum sound level, LAeq(125ms) (dBA): 119.08

Bridge Structure Number: BSN85

Name: ROSEBANK BRIDGE NO2

Route Position: 16 7 0

Direction: Increasing

Joint Type: Rubber extrusion unreinforced

Road Surface Type: Asphalt

Test Details:

Operator: I.Kvatch

Date: 15-05-13

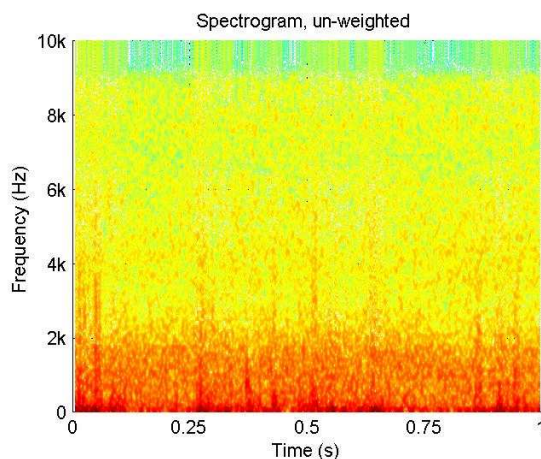
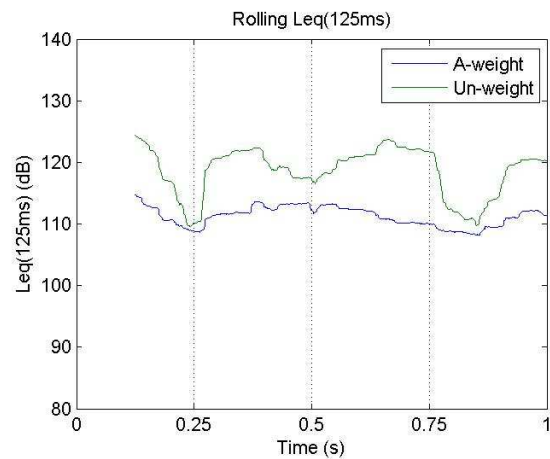
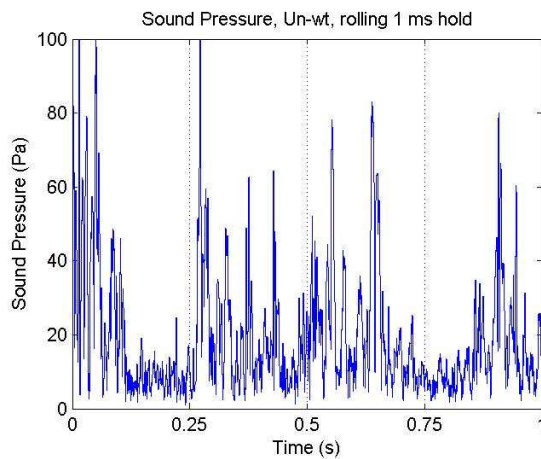
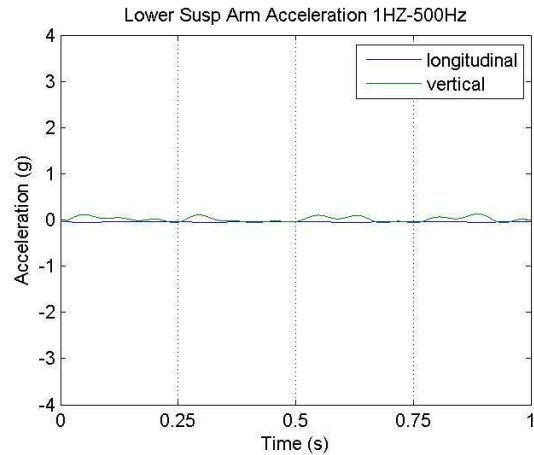
Time: 11-12-54

Marker No: 1

Latitude: -36.871943

Longitude: 174.693128

Speed (km/h): 78.5



Acceleration – Maximum vertical (g): 0.13
Acceleration – Maximum longitudinal (g): 0.06
Displacement – Maximum vertical (mm): 1.31
Displacement – Maximum longitudinal (mm): 0.60
Noise Peak Sound Pressure (Pa): 124.83
Average sound level, Leq(1s) (dB): 120.56
Average sound level, LAeq(1s) (dB): 111.94
Maximum sound level, Leq(125ms) (dB): 124.43
Maximum sound level, LAeq(125ms) (dBA): 114.88

Bridge Structure Number: BSN85

Name: ROSEBANK BRIDGE NO2

Route Position: 16 7 0

Direction: Increasing

Joint Type: Rubber extrusion unreinforced

Road Surface Type: Asphalt

Test Details:

Operator: I.Kvatch

Date: 15-05-13

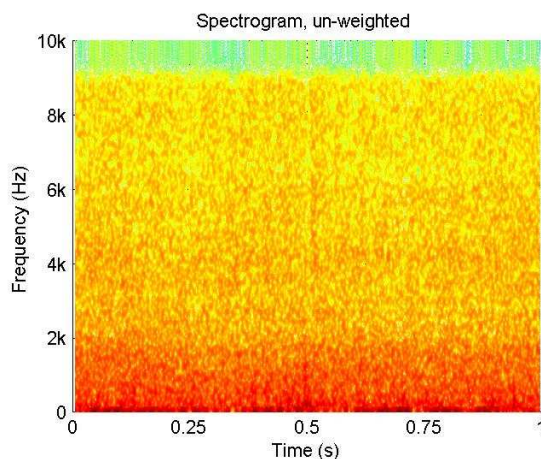
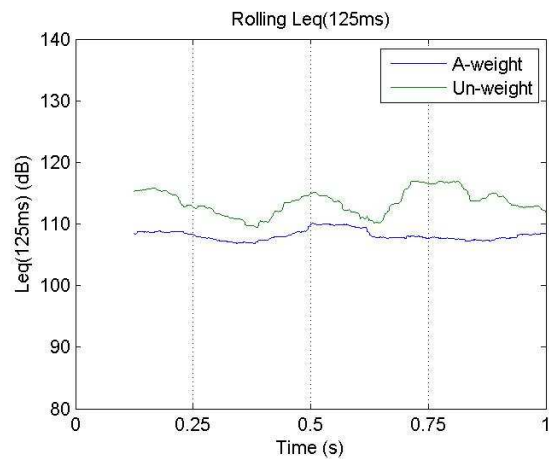
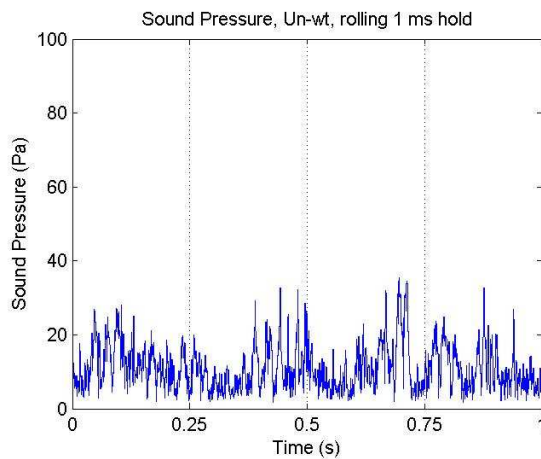
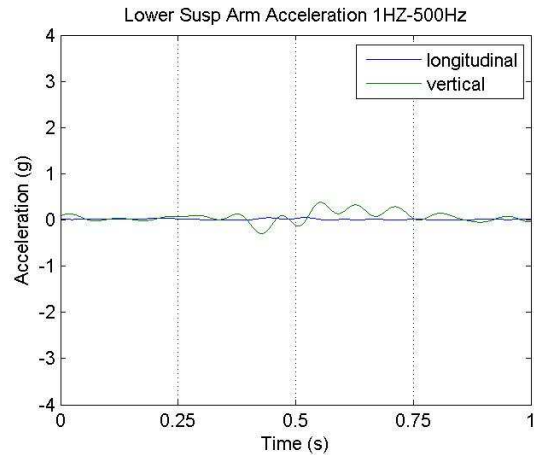
Time: 11-13-34

Marker No: 1

Latitude: -36.861443

Longitude: 174.662275

Speed (km/h): 79.3



Acceleration – Maximum vertical (g): 0.38
Acceleration – Maximum longitudinal (g): 0.05
Displacement – Maximum vertical (mm): 3.89
Displacement – Maximum longitudinal (mm): 0.52
Noise Peak Sound Pressure (Pa): 35.43
Average sound level, Leq(1s) (dB): 113.83
Average sound level, LAeq(1s) (dB): 108.26
Maximum sound level, Leq(125ms) (dB): 116.97
Maximum sound level, LAeq(125ms) (dBA): 110.16

Bridge Structure Number: BSN85

Name: ROSEBANK BRIDGE NO2

Route Position: 16 0 8.22

Direction: Increasing

Joint Type: Rubber extrusion unreinforced

Road Surface Type: Asphalt

Test Details:

Operator: I.Kvatch

Date: 15-05-13

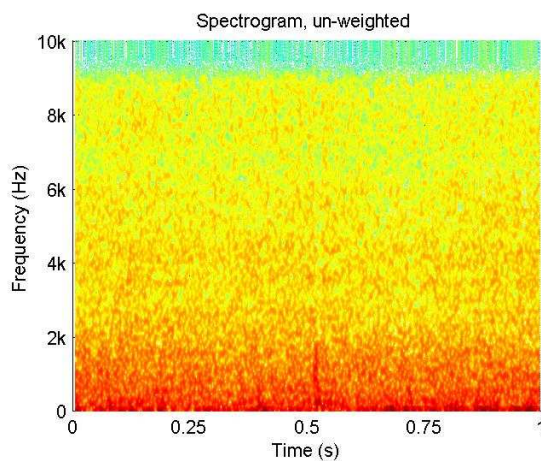
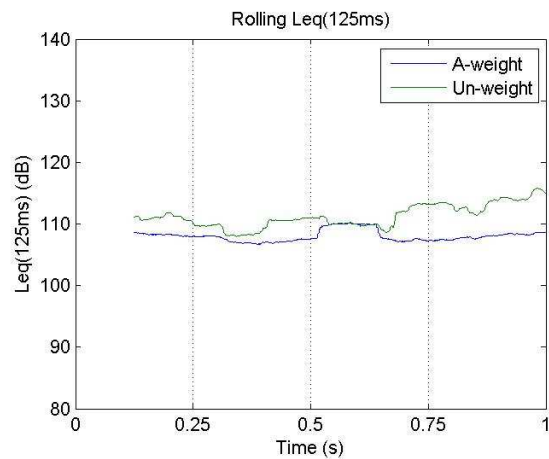
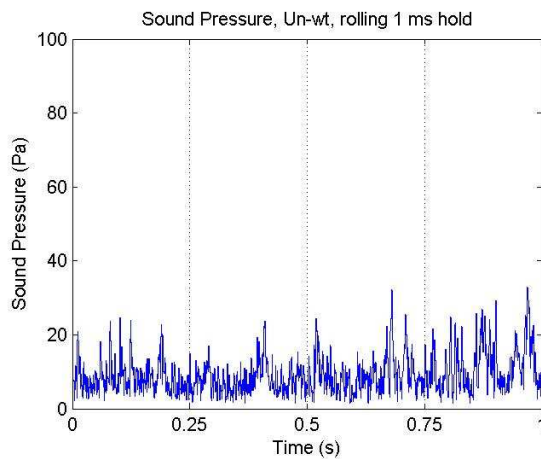
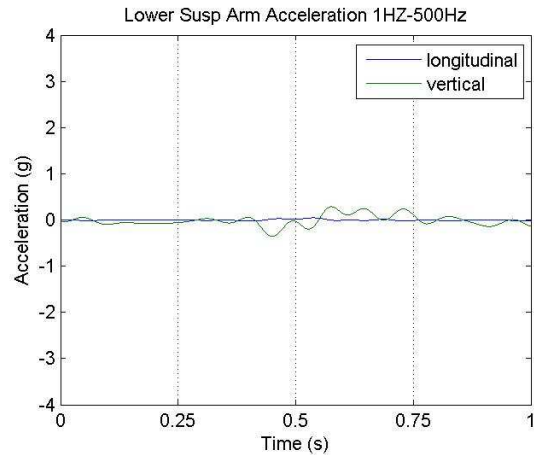
Time: 11:31:20

Marker No: 1

Latitude: -36.861402

Longitude: 174.662225

Speed (km/h): 80.2



Acceleration – Maximum vertical (g): 0.36
Acceleration – Maximum longitudinal (g): 0.05
Displacement – Maximum vertical (mm): 3.61
Displacement – Maximum longitudinal (mm): 0.48
Noise Peak Sound Pressure (Pa): 33.00
Average sound level, Leq(1s) (dB): 111.94
Average sound level, LAeq(1s) (dB): 108.21
Maximum sound level, Leq(125ms) (dB): 115.78
Maximum sound level, LAeq(125ms) (dBA): 110.13

Bridge Structure Number: BSN85

Name: ROSEBANK BRIDGE NO2

Route Position: 16 7 0

Direction: Increasing

Joint Type: Rubber extrusion unreinforced

Road Surface Type: Asphalt

Test Details:

Operator: I.Kvatch

Date: 15-05-13

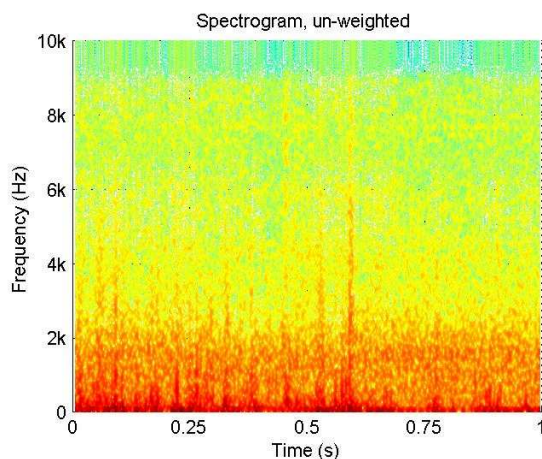
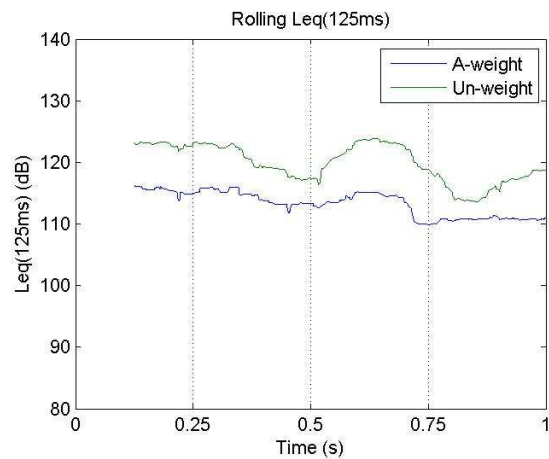
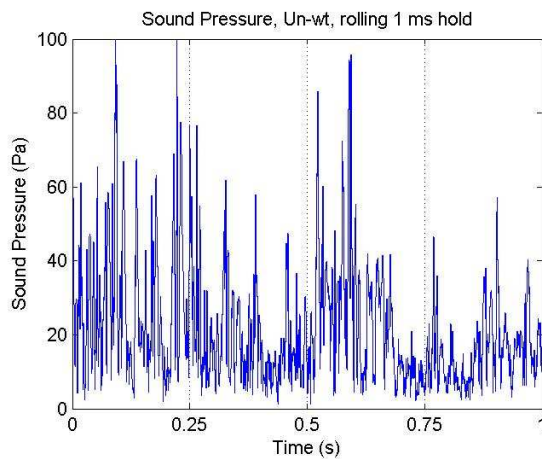
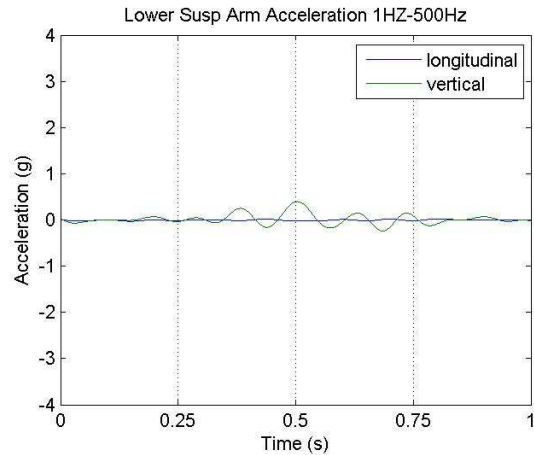
Time: 11-12-54

Marker No: 2

Latitude: -36.871957

Longitude: 174.692390

Speed (km/h): 79.1



Acceleration – Maximum vertical (g): 0.40
Acceleration – Maximum longitudinal (g): 0.03
Displacement – Maximum vertical (mm): 4.03
Displacement – Maximum longitudinal (mm): 0.30
Noise Peak Sound Pressure (Pa): 118.28
Average sound level, Leq(1s) (dB): 120.85
Average sound level, LAeq(1s) (dB): 113.83
Maximum sound level, Leq(125ms) (dB): 123.89
Maximum sound level, LAeq(125ms) (dBA): 116.42

Bridge Structure Number: BSN85

Name: ROSEBANK BRIDGE NO2

Route Position: 16 0 8.22

Direction: Increasing

Joint Type: Rubber extrusion unreinforced

Road Surface Type: Asphalt

Test Details:

Operator: I.Kvatch

Date: 15-05-13

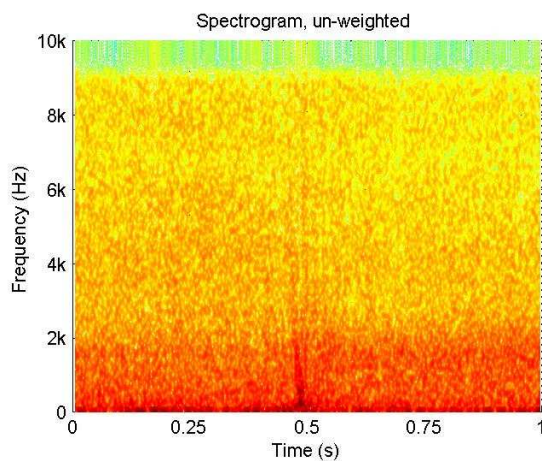
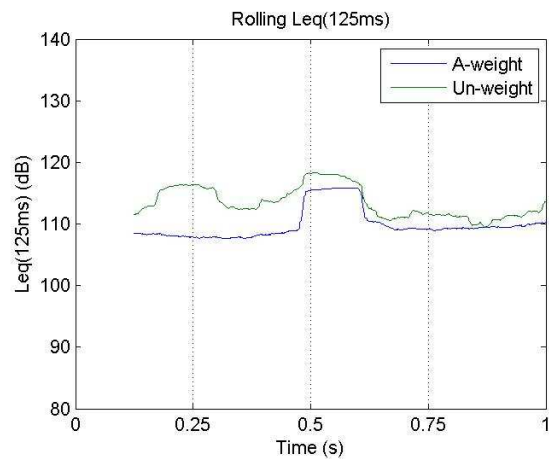
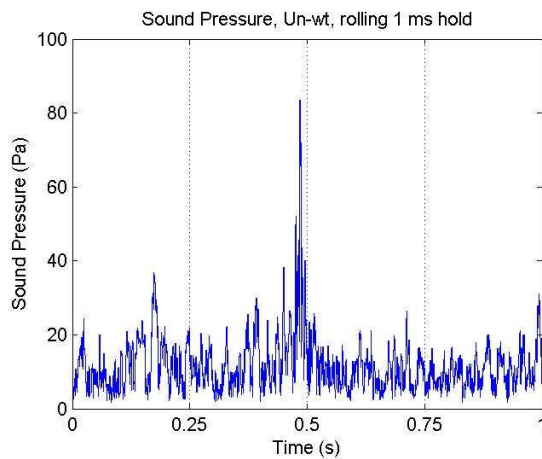
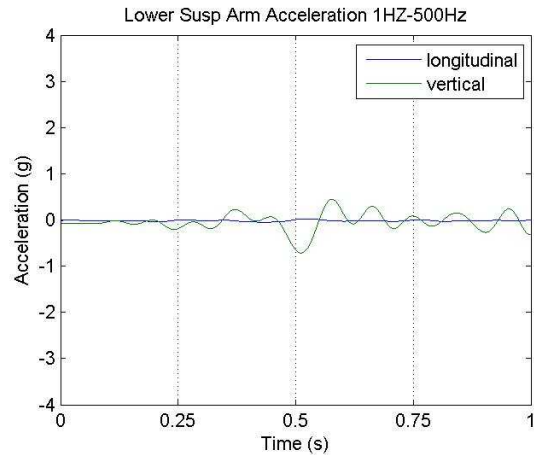
Time: 11:31:20

Marker No: 2

Latitude: -36.861120

Longitude: 174.661567

Speed (km/h): 79.8



Acceleration – Maximum vertical (g): 0.72
Acceleration – Maximum longitudinal (g): 0.05
Displacement – Maximum vertical (mm): 7.26
Displacement – Maximum longitudinal (mm): 0.47
Noise Peak Sound Pressure (Pa): 83.56
Average sound level, Leq(1s) (dB): 114.11
Average sound level, LAeq(1s) (dB): 110.67
Maximum sound level, Leq(125ms) (dB): 118.32
Maximum sound level, LAeq(125ms) (dBA): 115.87

Bridge Structure Number: BSN85

Name: ROSEBANK BRIDGE NO2

Route Position: 16 0 8.22

Direction: Increasing

Joint Type: Rubber extrusion unreinforced

Road Surface Type: Asphalt

Test Details:

Operator: I.Kvatch

Date: 15-05-13

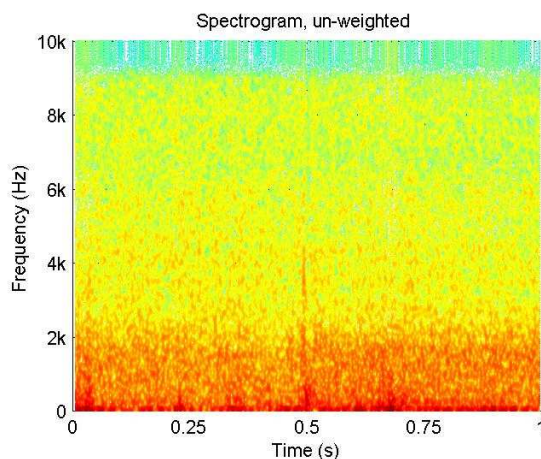
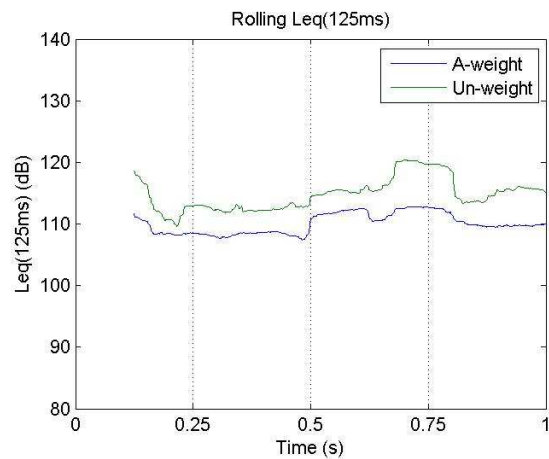
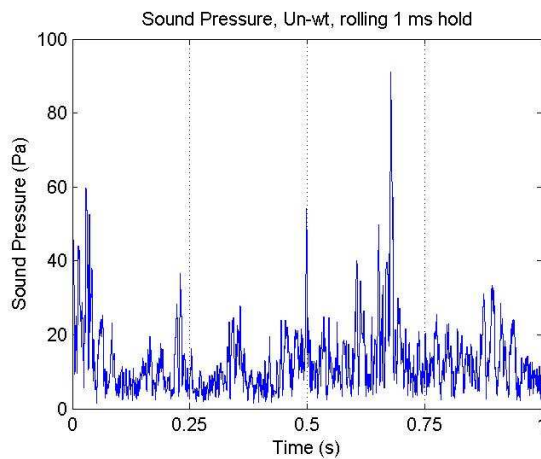
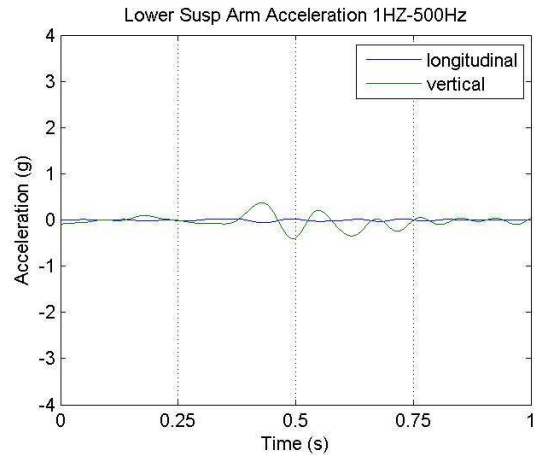
Time: 11:31:20

Marker No: 3

Latitude: -36.860868

Longitude: 174.660890

Speed (km/h): 79.6



Acceleration – Maximum vertical (g): 0.41
Acceleration – Maximum longitudinal (g): 0.06
Displacement – Maximum vertical (mm): 4.14
Displacement – Maximum longitudinal (mm): 0.64
Noise Peak Sound Pressure (Pa): 91.20
Average sound level, Leq(1s) (dB): 116.05
Average sound level, LAeq(1s) (dB): 110.54
Maximum sound level, Leq(125ms) (dB): 120.35
Maximum sound level, LAeq(125ms) (dBA): 112.82

Bridge Structure Number: BSN104

Name: WHAU RIVER BRIDGE No.2

Route Position: 16 7 3.5

Direction: Increasing

Joint Type: Rubber extrusion unreinforced

Road Surface Type: Asphalt

Test Details:

Operator: I.Kvatch

Date: 15-05-13

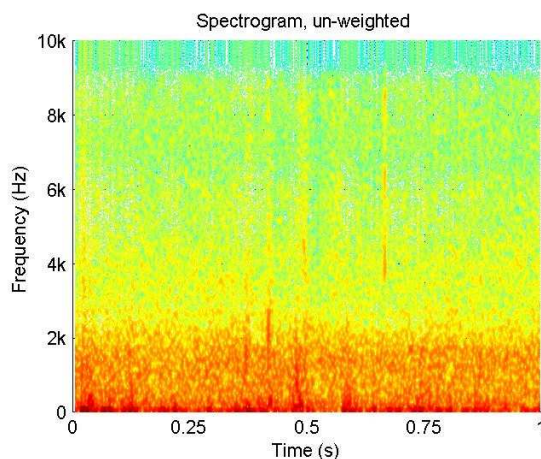
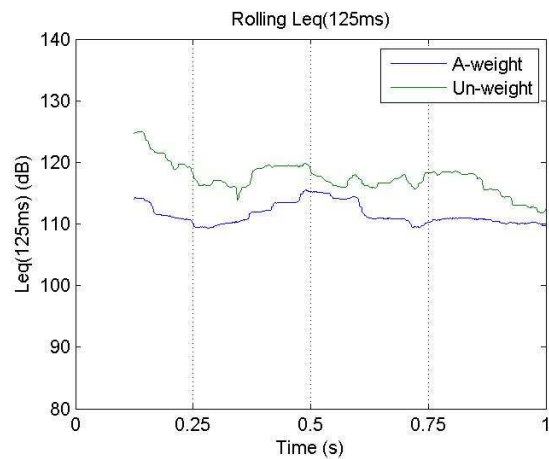
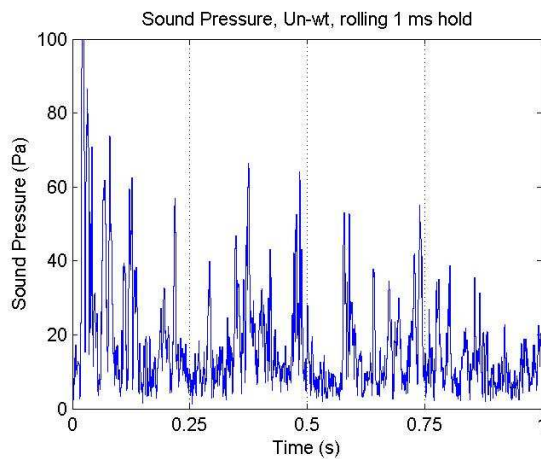
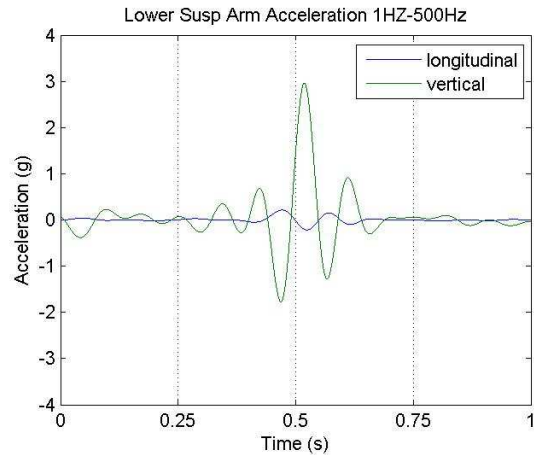
Time: 11:34:26

Marker No: 1

Latitude: -36.853180

Longitude: 174.641503

Speed (km/h): 80.0



Acceleration – Maximum vertical (g): 2.96
Acceleration – Maximum longitudinal (g): 0.22
Displacement – Maximum vertical (mm): 30.06
Displacement – Maximum longitudinal (mm): 2.19
Noise Peak Sound Pressure (Pa): 104.61
Average sound level, Leq(1s) (dB): 119.16
Average sound level, LAeq(1s) (dB): 112.06
Maximum sound level, Leq(125ms) (dB): 125.01
Maximum sound level, LAeq(125ms) (dBA): 115.52

Bridge Structure Number: BSN104

Name: WHAU RIVER BRIDGE No.2

Route Position: 16 7 3.5

Direction: Increasing

Joint Type: Rubber extrusion unreinforced

Road Surface Type: Asphalt

Test Details:

Operator: I.Kvatch

Date: 15-05-13

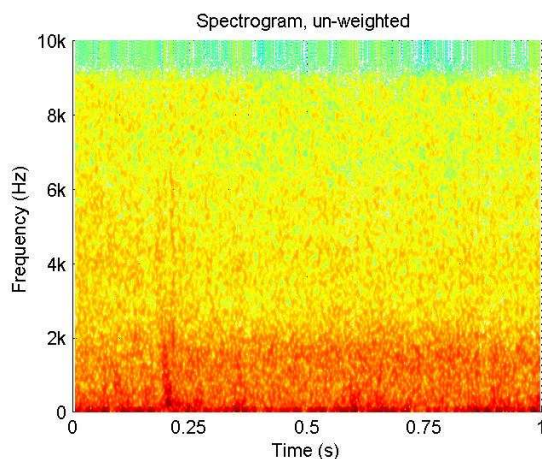
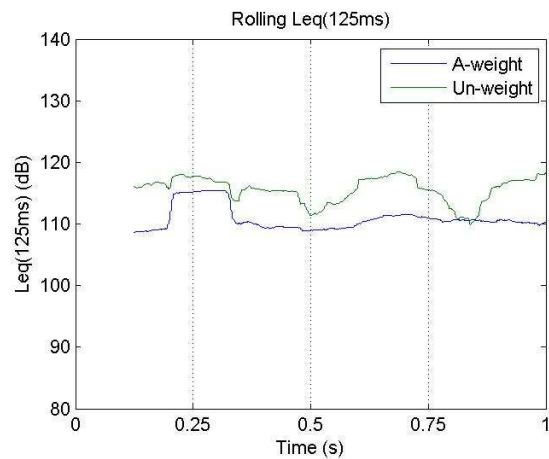
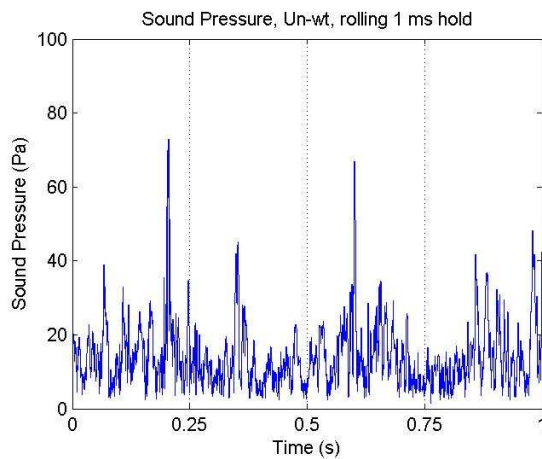
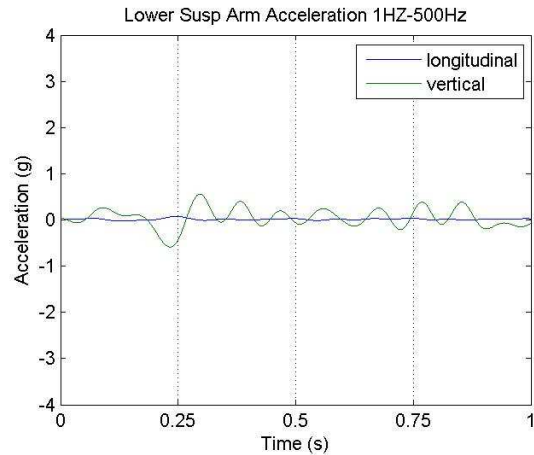
Time: 11-13-34

Marker No: 2

Latitude: -36.861075

Longitude: 174.661415

Speed (km/h): 78.7



Acceleration – Maximum vertical (g): 0.59
Acceleration – Maximum longitudinal (g): 0.07
Displacement – Maximum vertical (mm): 5.94
Displacement – Maximum longitudinal (mm): 0.76
Noise Peak Sound Pressure (Pa): 72.97
Average sound level, Leq(1s) (dB): 116.21
Average sound level, LAeq(1s) (dB): 111.16
Maximum sound level, Leq(125ms) (dB): 118.43
Maximum sound level, LAeq(125ms) (dBA): 115.41

Bridge Structure Number: BSN104

Name: WHAU RIVER BRIDGE No.2

Route Position: 16 7 3.5

Direction: Increasing

Joint Type: Rubber extrusion unreinforced

Road Surface Type: Asphalt

Test Details:

Operator: I.Kvatch

Date: 15-05-13

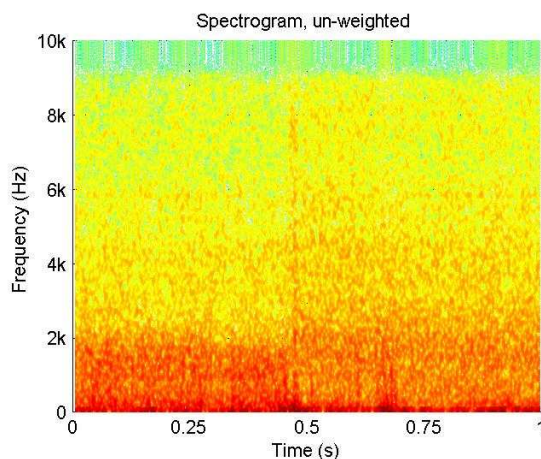
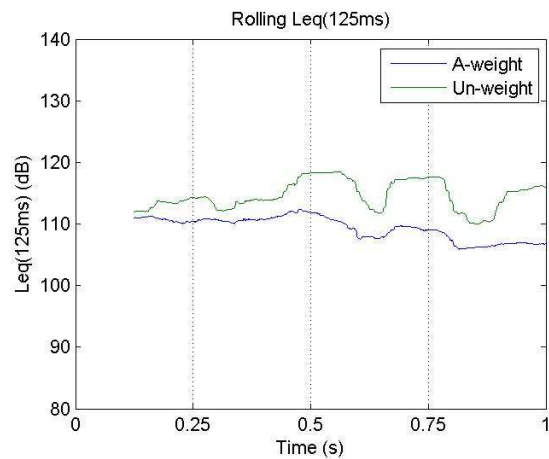
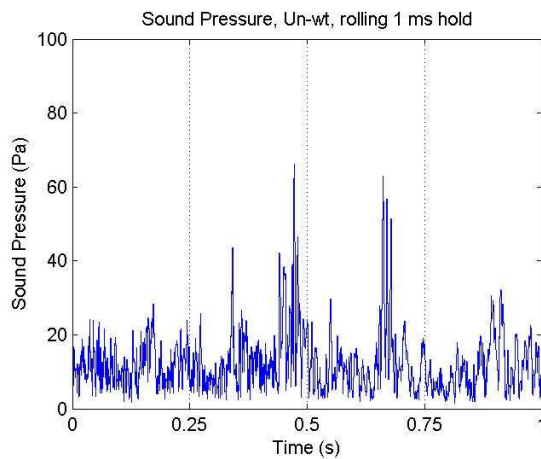
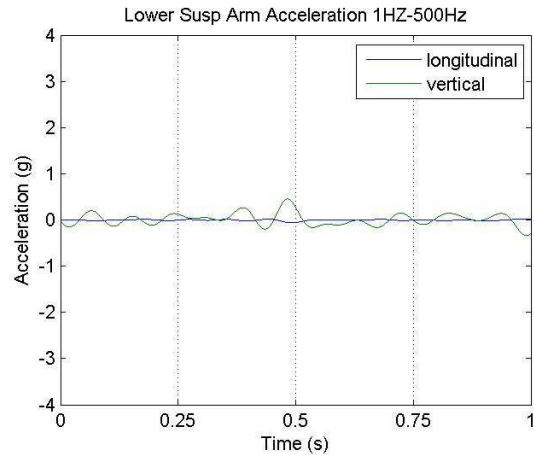
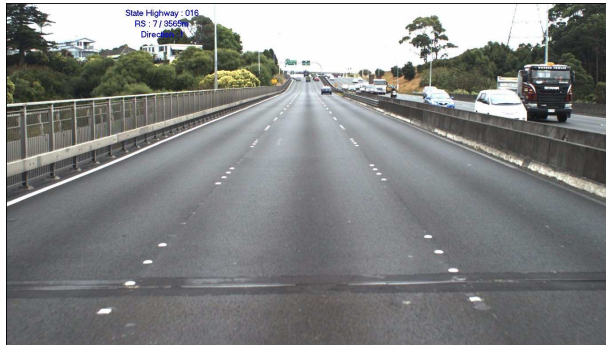
Time: 11:34:26

Marker No: 2

Latitude: -36.852648

Longitude: 174.640447

Speed (km/h): 79.8



Acceleration – Maximum vertical (g): 0.45
Acceleration – Maximum longitudinal (g): 0.06
Displacement – Maximum vertical (mm): 4.61
Displacement – Maximum longitudinal (mm): 0.62
Noise Peak Sound Pressure (Pa): 66.21
Average sound level, Leq(1s) (dB): 115.14
Average sound level, LAeq(1s) (dB): 109.66
Maximum sound level, Leq(125ms) (dB): 118.47
Maximum sound level, LAeq(125ms) (dBA): 112.39

Bridge Structure Number: BSN104

Name: WHAU RIVER BRIDGE No.2

Route Position: 16 7 3.5

Direction: Increasing

Joint Type: Rubber extrusion unreinforced

Road Surface Type: Asphalt

Test Details:

Operator: I.Kvatch

Date: 15-05-13

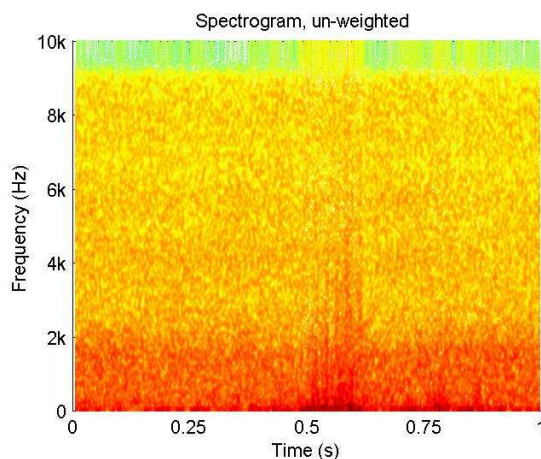
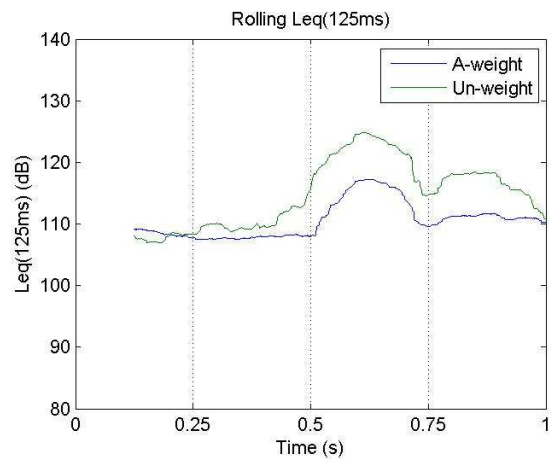
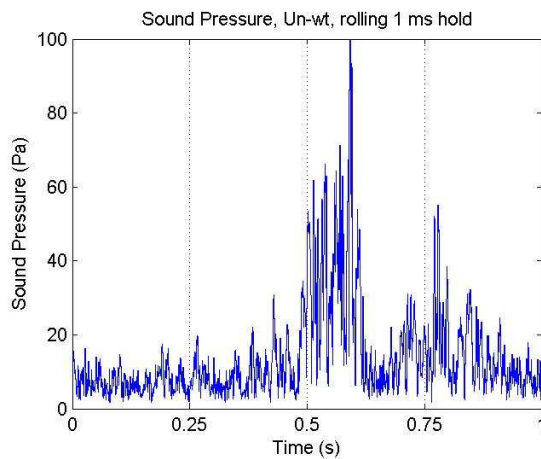
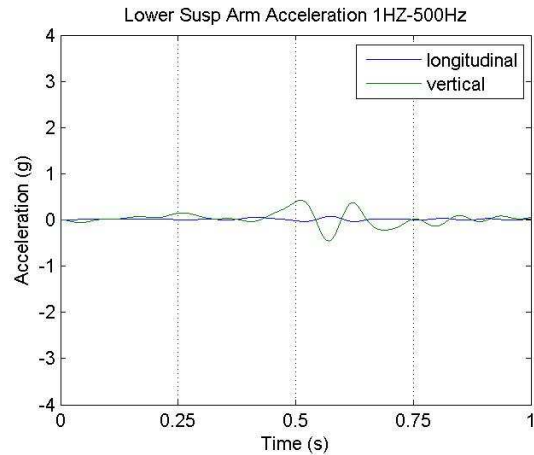
Time: 11-13-34

Marker No: 3

Latitude: -36.860913

Longitude: 174.660968

Speed (km/h): 78.5



Acceleration – Maximum vertical (g): 0.45
Acceleration – Maximum longitudinal (g): 0.08
Displacement – Maximum vertical (mm): 4.59
Displacement – Maximum longitudinal (mm): 0.81
Noise Peak Sound Pressure (Pa): 123.73
Average sound level, Leq(1s) (dB): 117.62
Average sound level, LAeq(1s) (dB): 111.52
Maximum sound level, Leq(125ms) (dB): 124.88
Maximum sound level, LAeq(125ms) (dBA): 117.22

Bridge Structure Number: BSN105

Name: WHAU RIVER BRIDGE No.1

Route Position: 16 7 3.5

Direction: Decreasing

Joint Type: Rubber extrusion unreinforced

Road Surface Type: Asphalt

Test Details:

Operator: I.Kvatch

Date: 15-05-13

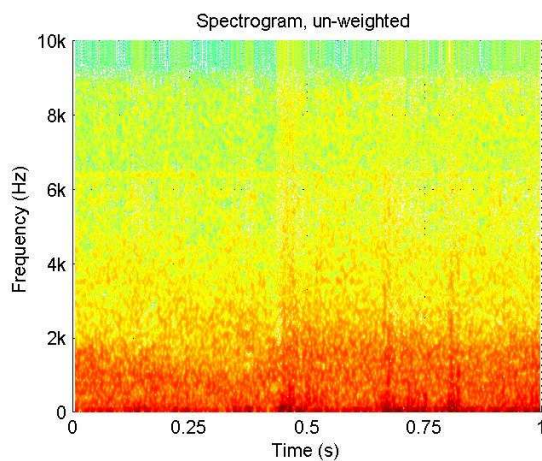
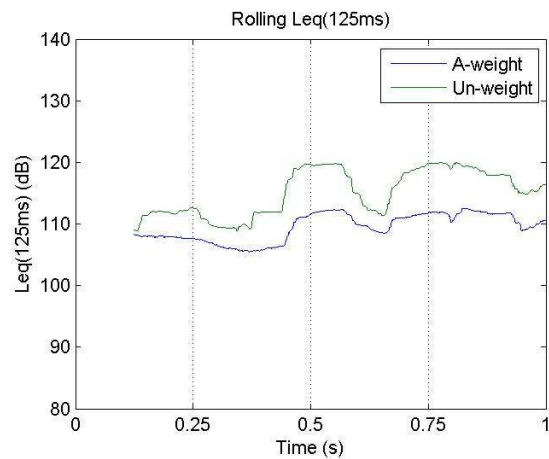
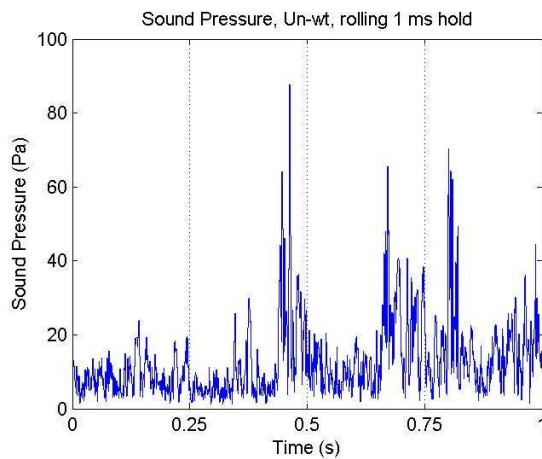
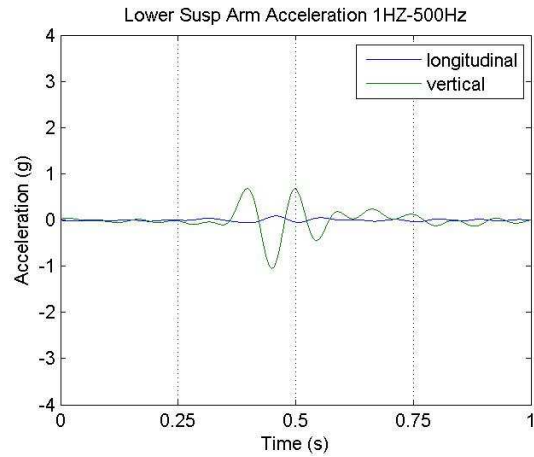
Time: 11-20-56

Marker No: 1

Latitude: -36.859177

Longitude: 174.656713

Speed (km/h): 82.6



Acceleration – Maximum vertical (g): 1.05
Acceleration – Maximum longitudinal (g): 0.09
Displacement – Maximum vertical (mm): 10.60
Displacement – Maximum longitudinal (mm): 0.87
Noise Peak Sound Pressure (Pa): 87.56
Average sound level, Leq(1s) (dB): 116.38
Average sound level, LAeq(1s) (dB): 110.03
Maximum sound level, Leq(125ms) (dB): 119.99
Maximum sound level, LAeq(125ms) (dBA): 112.54

Bridge Structure Number: BSN105

Name: WHAU RIVER BRIDGE No.1

Route Position: 16 7 3.5

Direction: Decreasing

Joint Type: Rubber extrusion unreinforced

Road Surface Type: Asphalt

Test Details:

Operator: I.Kvatch

Date: 15-05-13

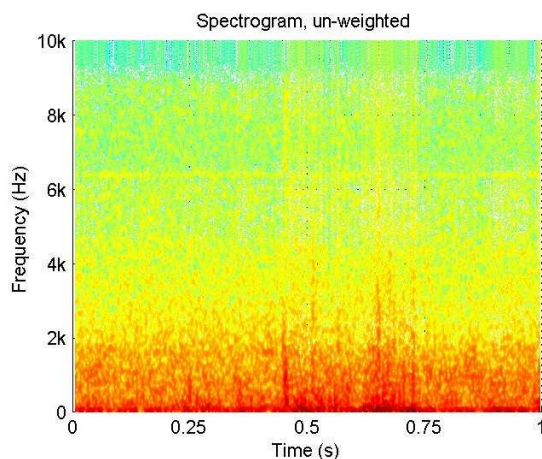
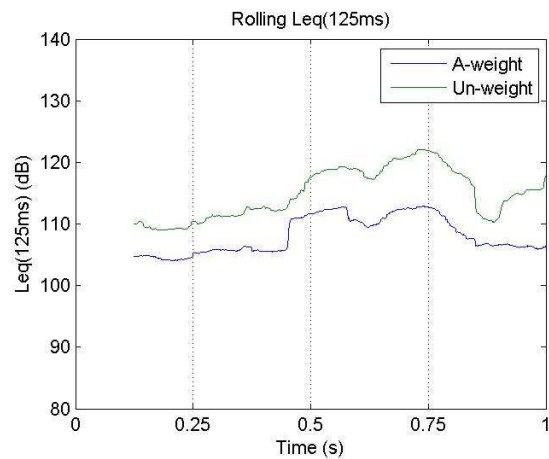
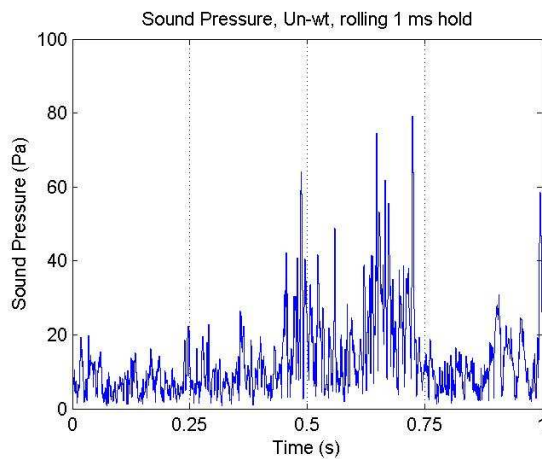
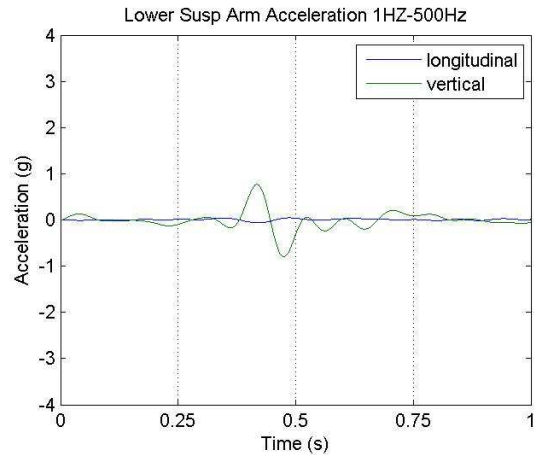
Time: 11-20-56

Marker No: 2

Latitude: -36.859520

Longitude: 174.657630

Speed (km/h): 80.8



Acceleration – Maximum vertical (g): 0.80
Acceleration – Maximum longitudinal (g): 0.06
Displacement – Maximum vertical (mm): 8.06
Displacement – Maximum longitudinal (mm): 0.63
Noise Peak Sound Pressure (Pa): 79.15
Average sound level, Leq(1s) (dB): 116.71
Average sound level, LAeq(1s) (dB): 108.88
Maximum sound level, Leq(125ms) (dB): 122.06
Maximum sound level, LAeq(125ms) (dBA): 112.85

Bridge Structure Number: BSN105

Name: WHAU RIVER BRIDGE No.1

Route Position: 16 7 3.5

Direction: Decreasing

Joint Type: Rubber extrusion unreinforced

Road Surface Type: Asphalt

Test Details:

Operator: I.Kvatch

Date: 15-05-13

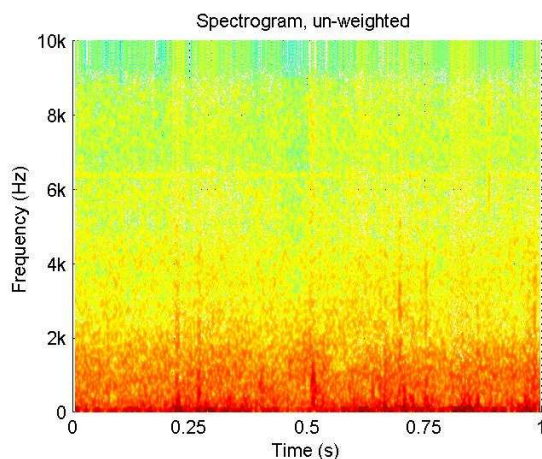
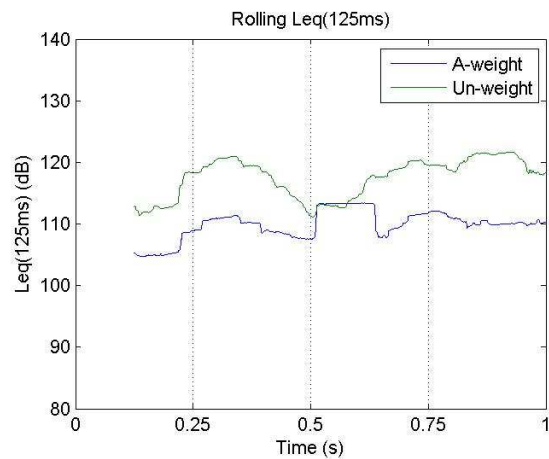
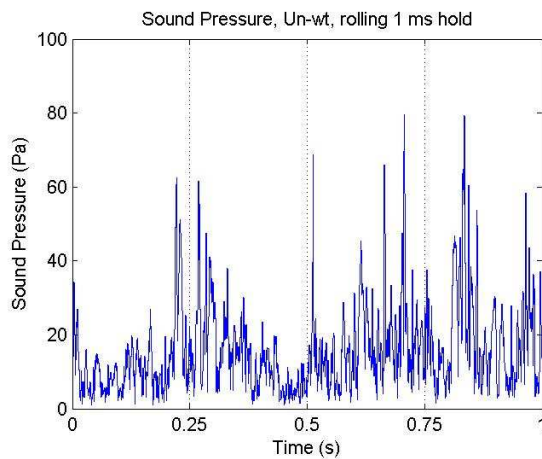
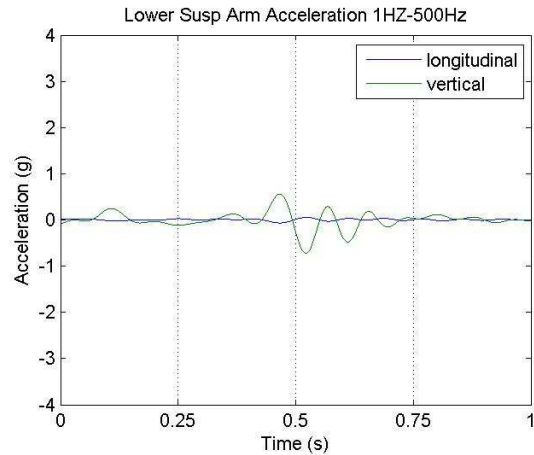
Time: 11-20-56

Marker No: 3

Latitude: -36.859772

Longitude: 174.658295

Speed (km/h): 78.7



Acceleration – Maximum vertical (g): 0.72
Acceleration – Maximum longitudinal (g): 0.07
Displacement – Maximum vertical (mm): 7.31
Displacement – Maximum longitudinal (mm): 0.67
Noise Peak Sound Pressure (Pa): 79.47
Average sound level, Leq(1s) (dB): 118.31
Average sound level, LAeq(1s) (dB): 110.28
Maximum sound level, Leq(125ms) (dB): 121.66
Maximum sound level, LAeq(125ms) (dBA): 113.40

Bridge Structure Number: BSN107

Name: INTERCHANGE BRIDGE

Route Position: 20 0 10.5

Direction: Increasing

Joint Type: Rubber extrusion + embedded steel plate

Road Surface Type: Asphalt

Test Details:

Operator: I.Kvatch

Date: 15-05-13

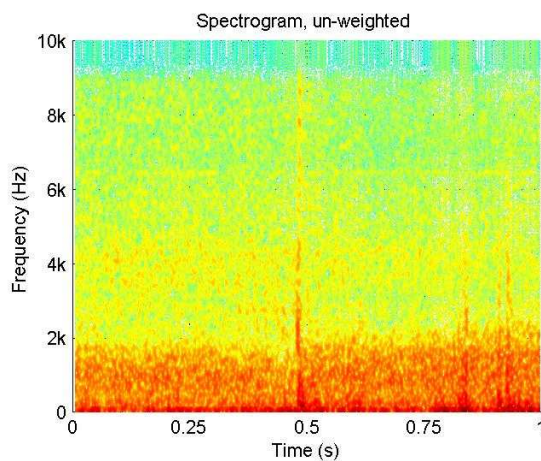
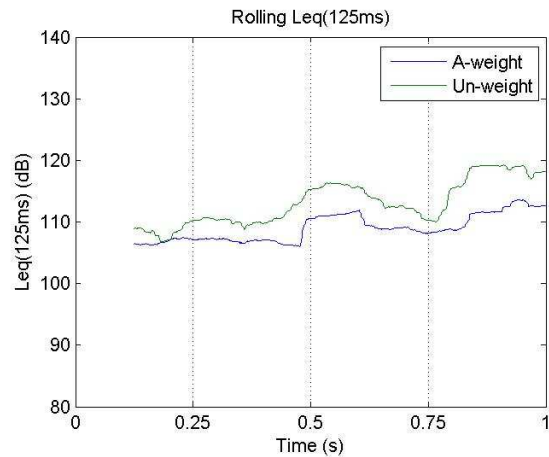
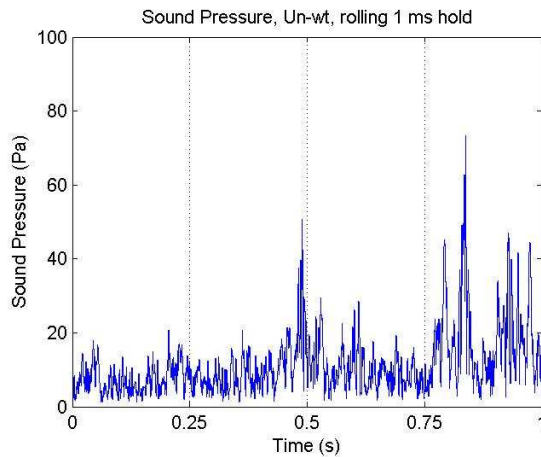
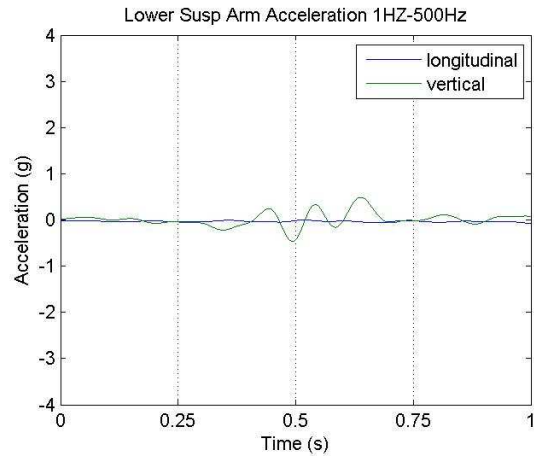
Time: 10-37-42

Marker No: 1

Latitude: -36.963330

Longitude: 174.800203

Speed (km/h): 78.2



Acceleration – Maximum vertical (g): 0.49
Acceleration – Maximum longitudinal (g): 0.07
Displacement – Maximum vertical (mm): 4.95
Displacement – Maximum longitudinal (mm): 0.69
Noise Peak Sound Pressure (Pa): 73.28
Average sound level, Leq(1s) (dB): 114.85
Average sound level, LAeq(1s) (dB): 109.65
Maximum sound level, Leq(125ms) (dB): 119.22
Maximum sound level, LAeq(125ms) (dBA): 113.58

Bridge Structure Number: BSN107

Name: INTERCHANGE BRIDGE

Route Position: 20 0 10.5

Direction: Increasing

Joint Type: Rubber extrusion + embedded steel plate

Road Surface Type: Asphalt

Test Details:

Operator: I.Kvatch

Date: 15-05-13

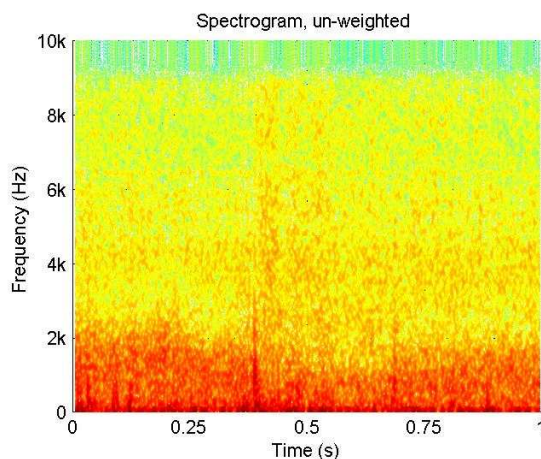
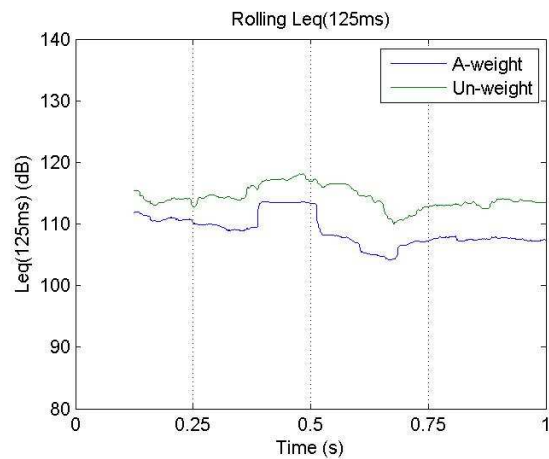
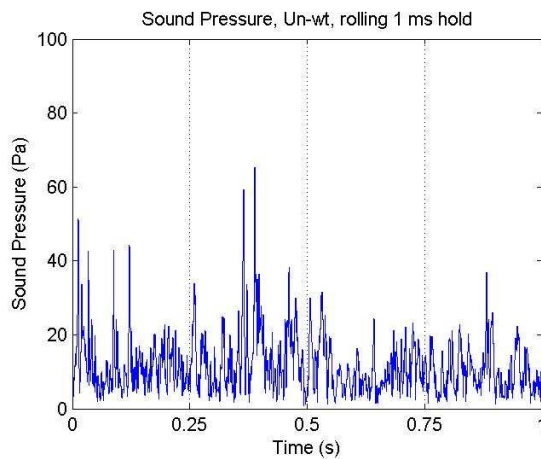
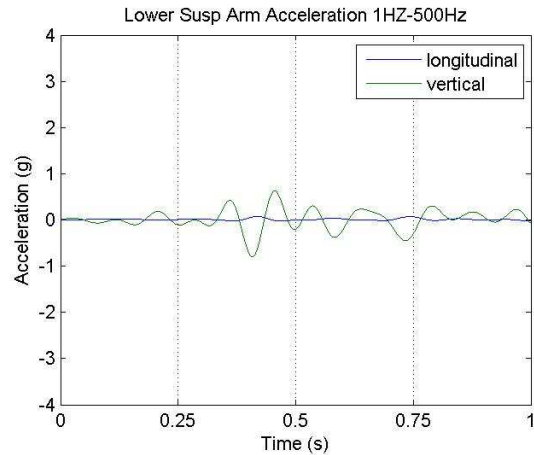
Time: 10-37-42

Marker No: 2

Latitude: -36.962292

Longitude: 174.799503

Speed (km/h): 79.3



Acceleration – Maximum vertical (g): 0.79
Acceleration – Maximum longitudinal (g): 0.07
Displacement – Maximum vertical (mm): 8.04
Displacement – Maximum longitudinal (mm): 0.76
Noise Peak Sound Pressure (Pa): 65.29
Average sound level, Leq(1s) (dB): 114.63
Average sound level, LAeq(1s) (dB): 109.77
Maximum sound level, Leq(125ms) (dB): 118.14
Maximum sound level, LAeq(125ms) (dBA): 113.64

Bridge Structure Number: BSN128

Name: MANGERE BRIDGE (SOUTHBOUND)

Route Position: 20 10 2.53

Direction: Decreasing

Joint Type: Steel finger joint with or without rubber

Road Surface Type: Asphalt

Test Details:

Operator: I.Kvatch

Date: 15-05-13

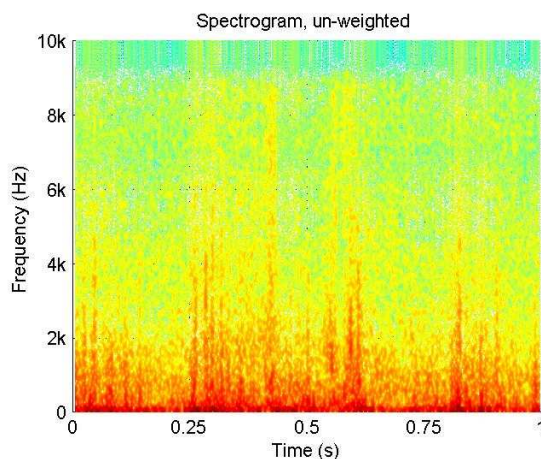
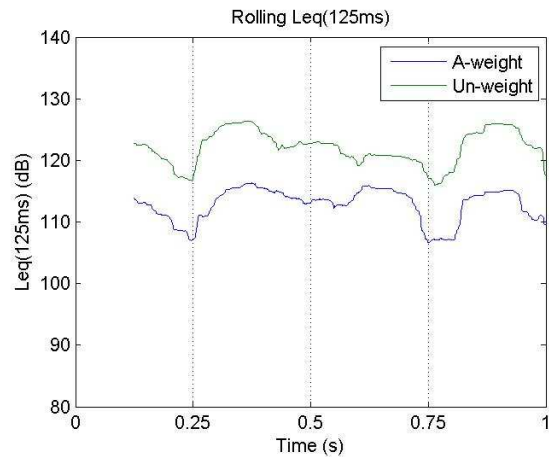
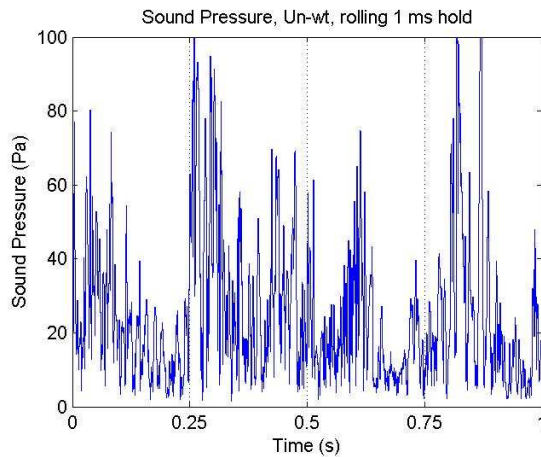
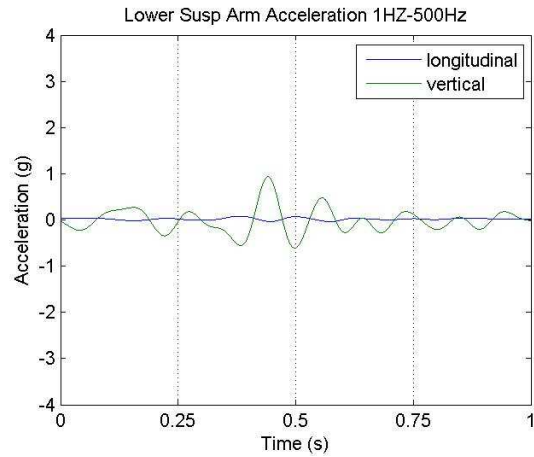
Time: 10-43-32

Marker No: 1

Latitude: -36.930877

Longitude: 174.786472

Speed (km/h): 80.6



Acceleration – Maximum vertical (g): 0.94
Acceleration – Maximum longitudinal (g): 0.08
Displacement – Maximum vertical (mm): 9.49
Displacement – Maximum longitudinal (mm): 0.83
Noise Peak Sound Pressure (Pa): 147.44
Average sound level, Leq(1s) (dB): 122.65
Average sound level, LAeq(1s) (dB): 113.42
Maximum sound level, Leq(125ms) (dB): 126.35
Maximum sound level, LAeq(125ms) (dBA): 116.31

Bridge Structure Number: BSN128

Name: MANGERE BRIDGE (SOUTHBOUND)

Route Position: 20 10 2.53

Direction: Decreasing

Joint Type: Steel finger joint with or without rubber

Road Surface Type: Asphalt

Test Details:

Operator: I.Kvatch

Date: 15-05-13

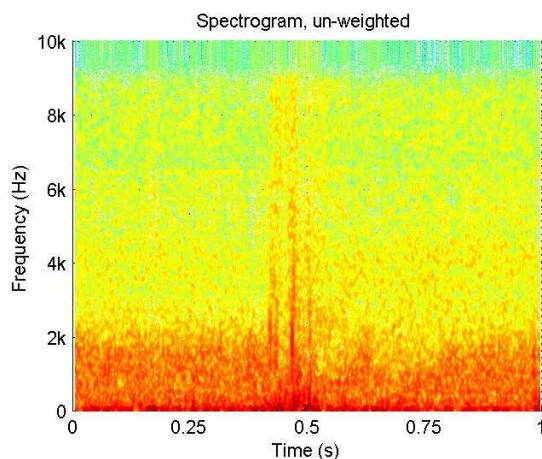
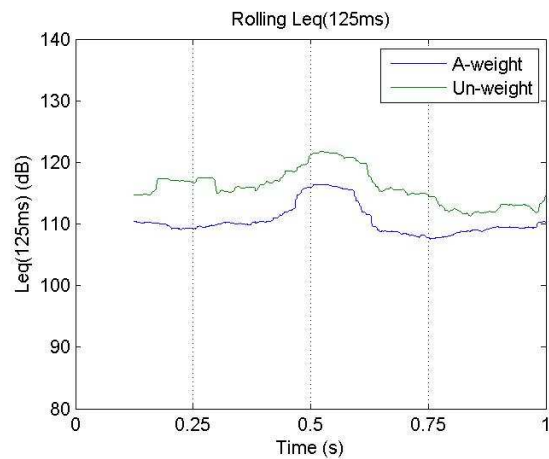
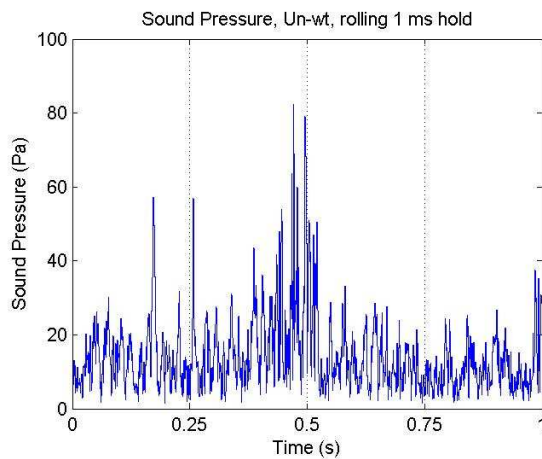
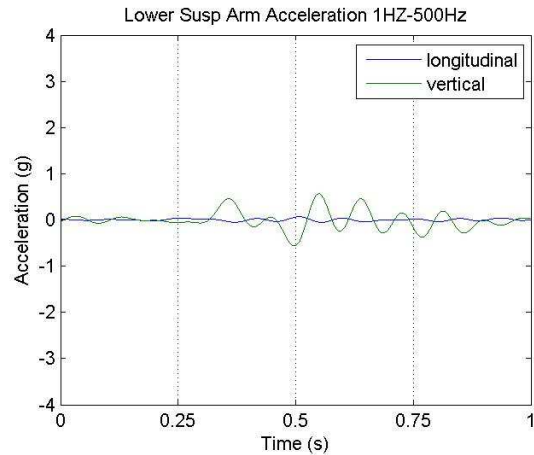
Time: 10-43-32

Marker No: 2

Latitude: -36.934723

Longitude: 174.789048

Speed (km/h): 80.4



Acceleration – Maximum vertical (g): 0.57
Acceleration – Maximum longitudinal (g): 0.07
Displacement – Maximum vertical (mm): 5.75
Displacement – Maximum longitudinal (mm): 0.70
Noise Peak Sound Pressure (Pa): 82.41
Average sound level, Leq(1s) (dB): 116.76
Average sound level, LAeq(1s) (dB): 111.29
Maximum sound level, Leq(125ms) (dB): 121.81
Maximum sound level, LAeq(125ms) (dBA): 116.41

Bridge Structure Number: BSN129

Name: MANGERE BRIDGE (NORTHBOUND)

Route Position: 20 10 2.53

Direction: Increasing

Joint Type: Rubber seals + vert. steel plates

Road Surface Type: Asphalt

Test Details:

Operator: I.Kvatch

Date: 15-05-13

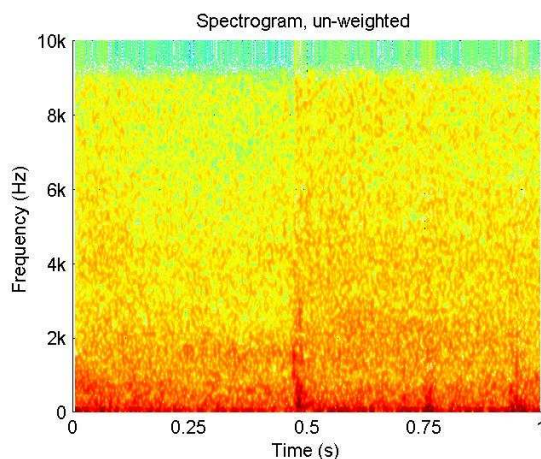
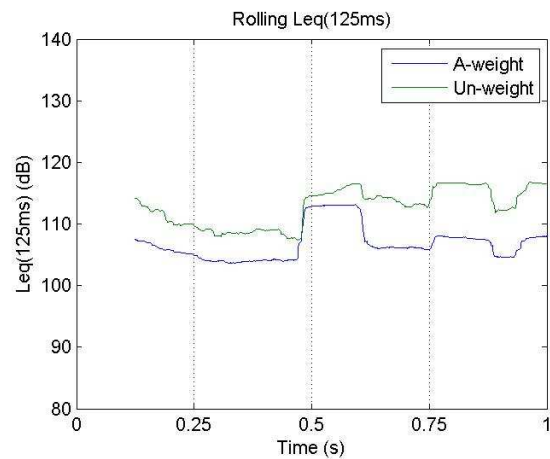
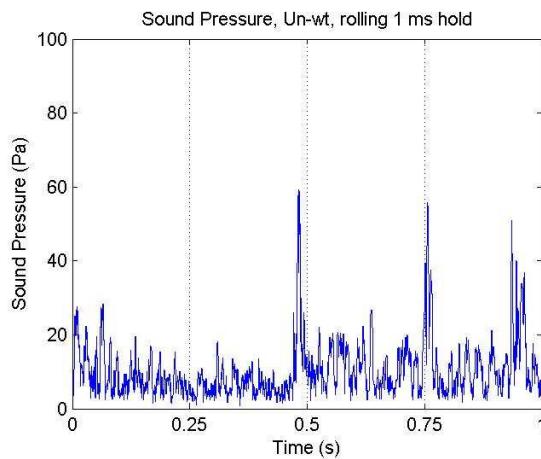
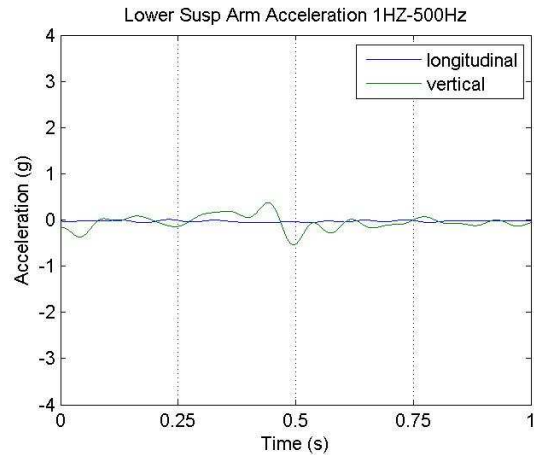
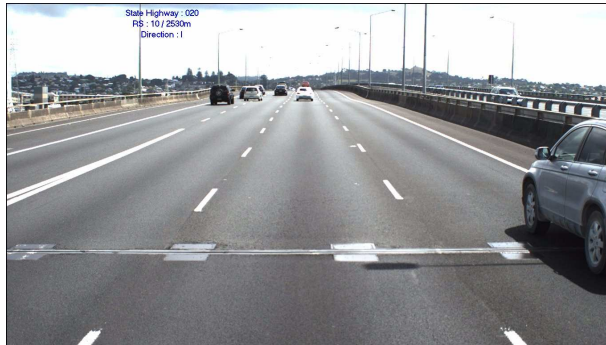
Time: 10-40-21

Marker No: 1

Latitude: -36.936635

Longitude: 174.789393

Speed (km/h): 79.8



Acceleration – Maximum vertical (g): 0.54
Acceleration – Maximum longitudinal (g): 0.06
Displacement – Maximum vertical (mm): 5.46
Displacement – Maximum longitudinal (mm): 0.65
Noise Peak Sound Pressure (Pa): 59.13
Average sound level, Leq(1s) (dB): 114.11
Average sound level, LAeq(1s) (dB): 107.99
Maximum sound level, Leq(125ms) (dB): 116.81
Maximum sound level, LAeq(125ms) (dBA): 113.12

Bridge Structure Number: BSN129

Name: MANGERE BRIDGE (NORTHBOUND)

Route Position: 20 10 2.53

Direction: Increasing

Joint Type: Rubber seals + vert. steel plates

Road Surface Type: Asphalt

Test Details:

Operator: I.Kvatch

Date: 15-05-13

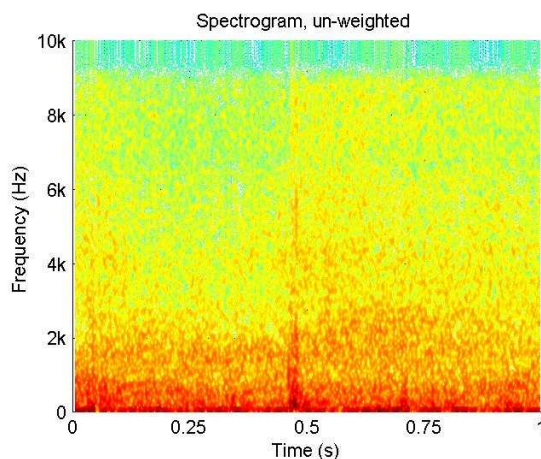
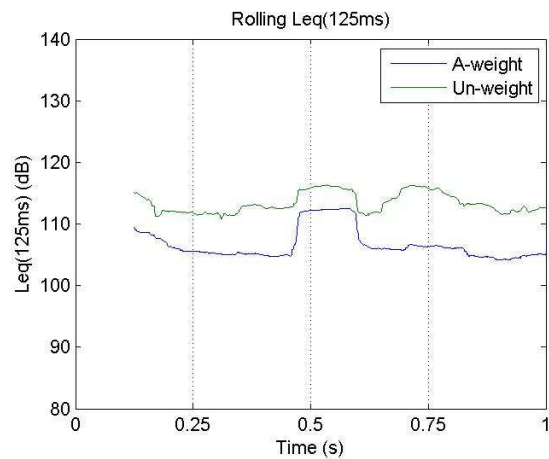
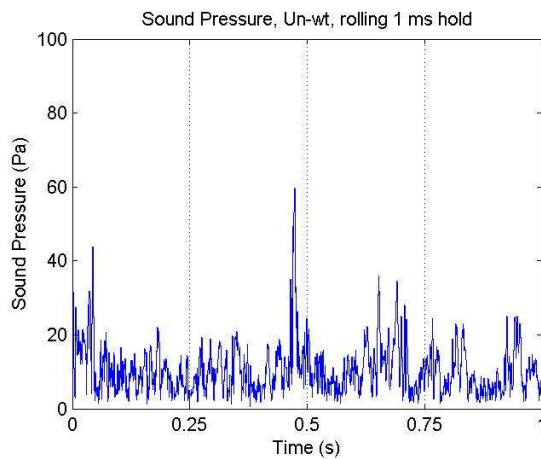
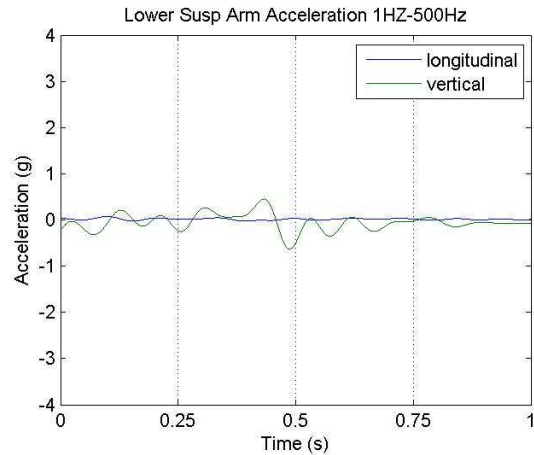
Time: 10-49-32

Marker No: 1

Latitude: -36.936682

Longitude: 174.789397

Speed (km/h): 78.9



Acceleration – Maximum vertical (g): 0.63
Acceleration – Maximum longitudinal (g): 0.07
Displacement – Maximum vertical (mm): 6.41
Displacement – Maximum longitudinal (mm): 0.76
Noise Peak Sound Pressure (Pa): 59.78
Average sound level, Leq(1s) (dB): 113.93
Average sound level, LAeq(1s) (dB): 107.68
Maximum sound level, Leq(125ms) (dB): 116.31
Maximum sound level, LAeq(125ms) (dBA): 112.50

Bridge Structure Number: BSN129

Name: MANGERE BRIDGE (NORTHBOUND)

Route Position: 20 10 2.53

Direction: Increasing

Joint Type: Rubber seals + vert. steel plates

Road Surface Type: Asphalt

Test Details:

Operator: I.Kvatch

Date: 15-05-13

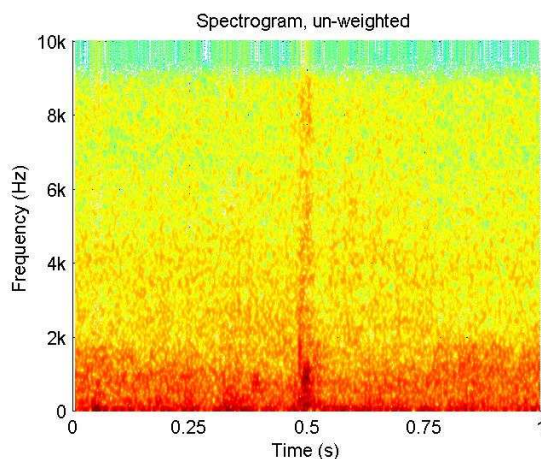
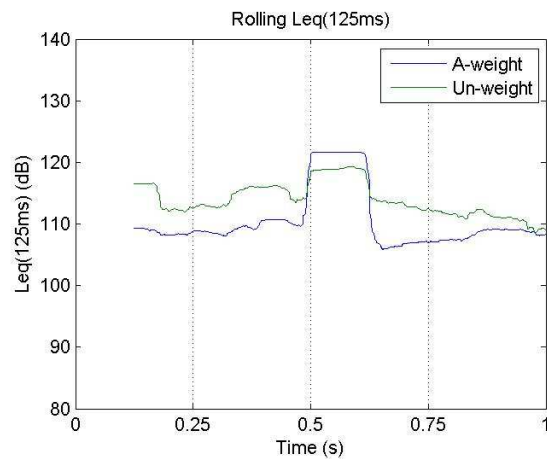
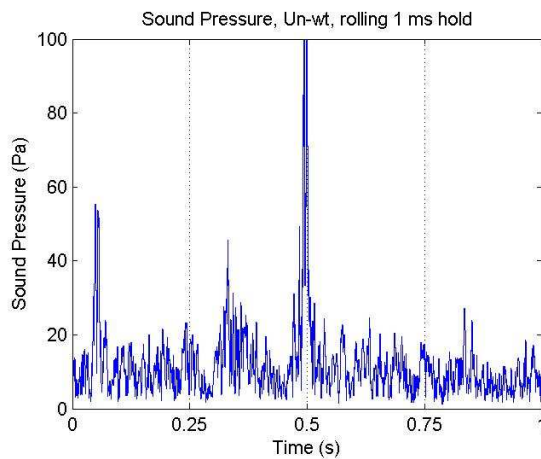
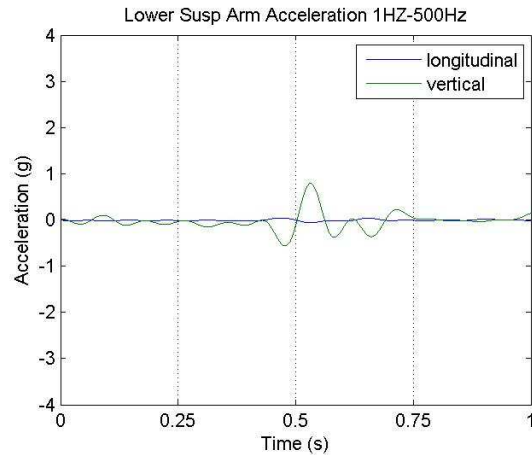
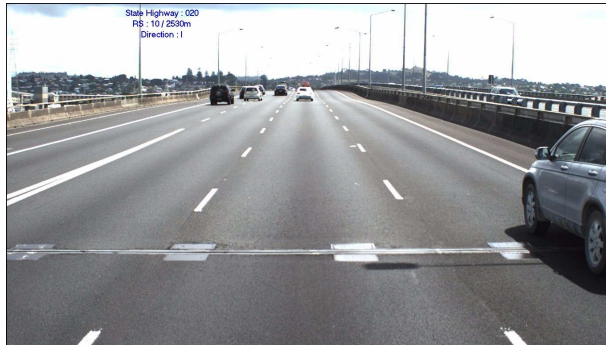
Time: 10-40-21

Marker No: 2

Latitude: -36.935863

Longitude: 174.789125

Speed (km/h): 80.0



Acceleration – Maximum vertical (g): 0.79
Acceleration – Maximum longitudinal (g): 0.06
Displacement – Maximum vertical (mm): 8.04
Displacement – Maximum longitudinal (mm): 0.60
Noise Peak Sound Pressure (Pa): 119.80
Average sound level, Leq(1s) (dB): 114.81
Average sound level, LAeq(1s) (dB): 113.95
Maximum sound level, Leq(125ms) (dB): 119.26
Maximum sound level, LAeq(125ms) (dBA): 121.70

Bridge Structure Number: BSN129

Name: MANGERE BRIDGE (NORTHBOUND)

Route Position: 20 10 2.53

Direction: Increasing

Joint Type: Rubber seals + vert. steel plates

Road Surface Type: Asphalt

Test Details:

Operator: I.Kvatch

Date: 15-05-13

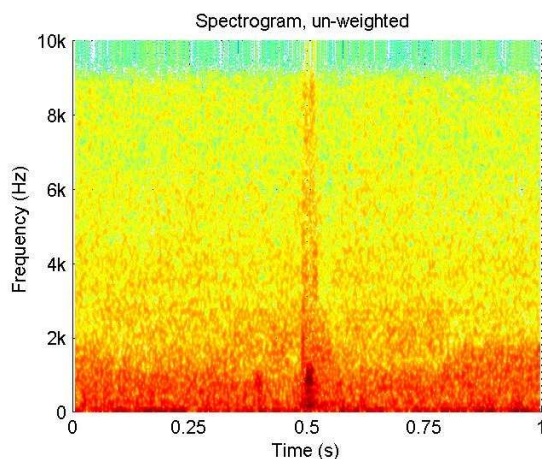
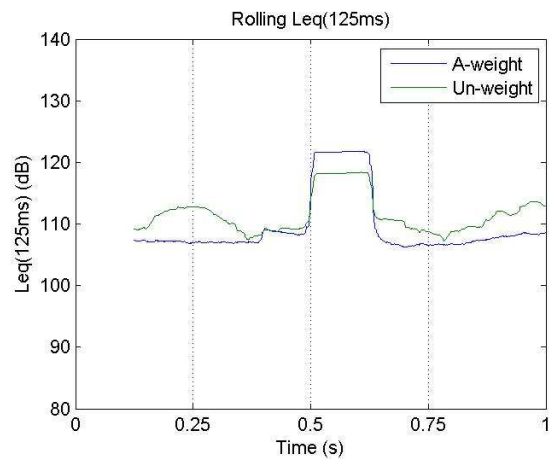
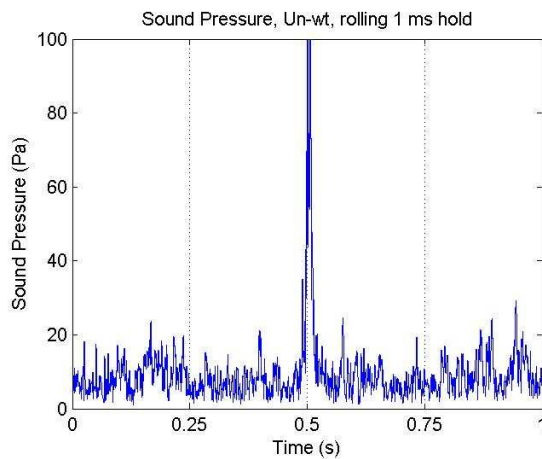
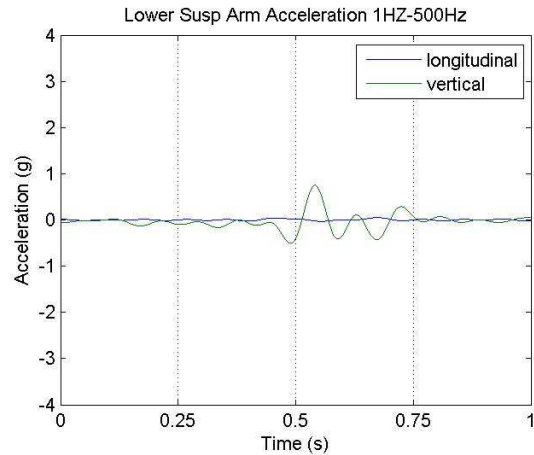
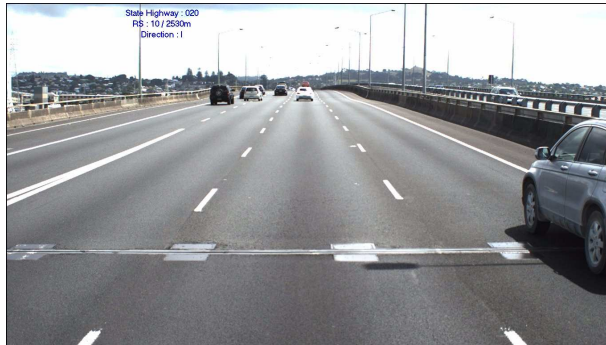
Time: 10-49-32

Marker No: 2

Latitude: -36.935920

Longitude: 174.789138

Speed (km/h): 78.9



Acceleration – Maximum vertical (g): 0.75
Acceleration – Maximum longitudinal (g): 0.05
Displacement – Maximum vertical (mm): 7.62
Displacement – Maximum longitudinal (mm): 0.51
Noise Peak Sound Pressure (Pa): 111.85
Average sound level, Leq(1s) (dB): 112.72
Average sound level, LAeq(1s) (dB): 113.72
Maximum sound level, Leq(125ms) (dB): 118.40
Maximum sound level, LAeq(125ms) (dBA): 121.73

Bridge Structure Number: BSN129

Name: MANGERE BRIDGE (NORTHBOUND)

Route Position: 20 10 2.53

Direction: Increasing

Joint Type: Rubber seals + vert. steel plates

Road Surface Type: Asphalt

Test Details:

Operator: I.Kvatch

Date: 15-05-13

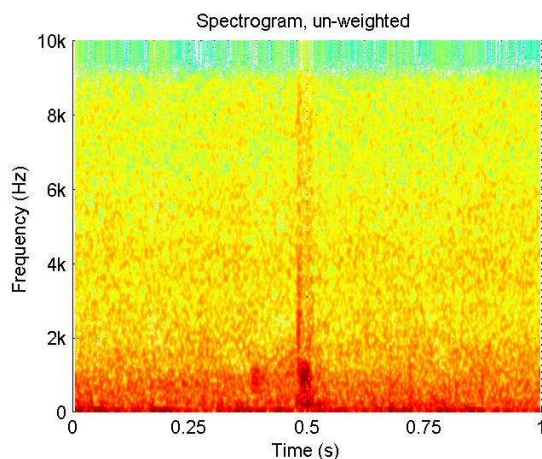
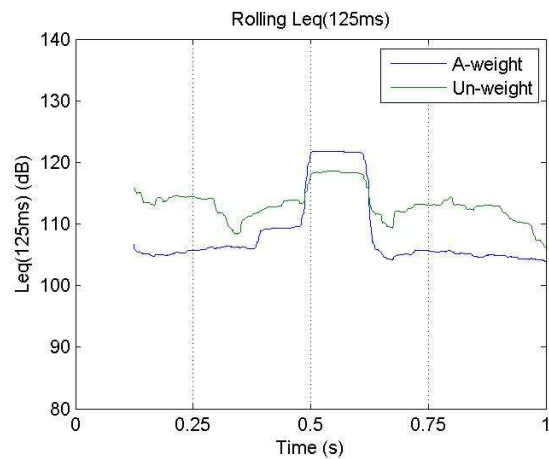
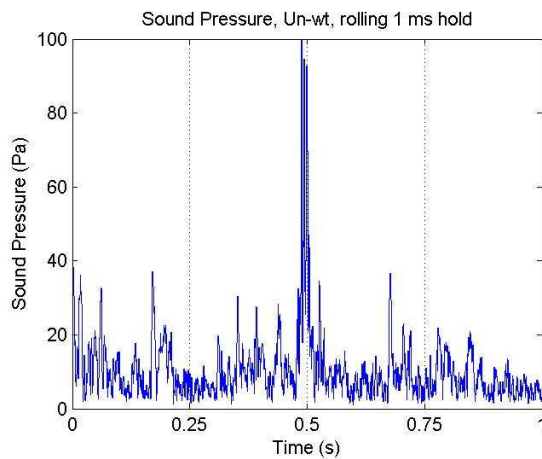
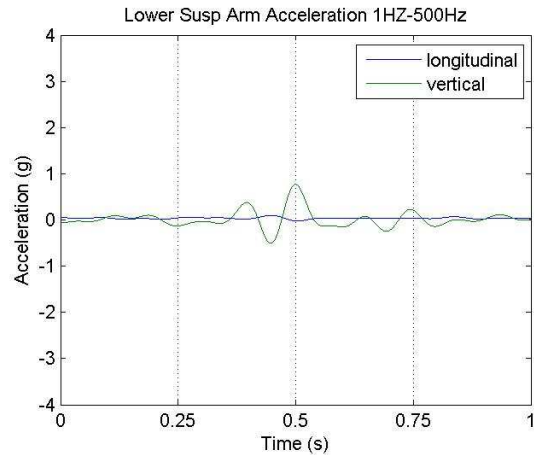
Time: 10-40-21

Marker No: 3

Latitude: -36.934313

Longitude: 174.788605

Speed (km/h): 80.8



Acceleration – Maximum vertical (g): 0.77
Acceleration – Maximum longitudinal (g): 0.10
Displacement – Maximum vertical (mm): 7.82
Displacement – Maximum longitudinal (mm): 0.99
Noise Peak Sound Pressure (Pa): 101.72
Average sound level, Leq(1s) (dB): 114.19
Average sound level, LAeq(1s) (dB): 113.47
Maximum sound level, Leq(125ms) (dB): 118.61
Maximum sound level, LAeq(125ms) (dBA): 121.80

Bridge Structure Number: BSN129

Name: MANGERE BRIDGE (NORTHBOUND)

Route Position: 20 10 2.53

Direction: Increasing

Joint Type: Rubber seals + vert. steel plates

Road Surface Type: Asphalt

Test Details:

Operator: I.Kvatch

Date: 15-05-13

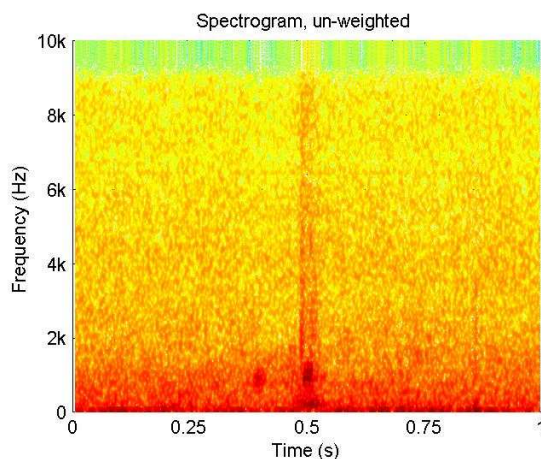
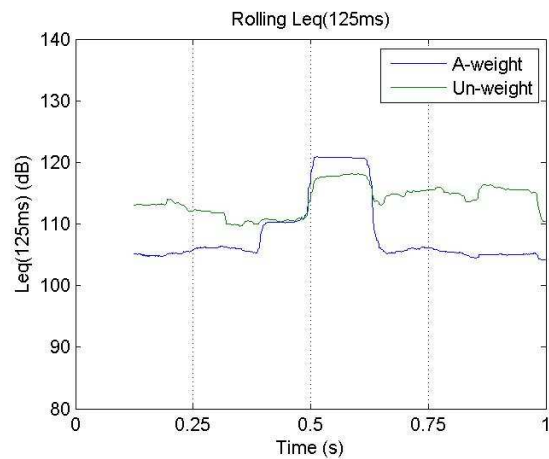
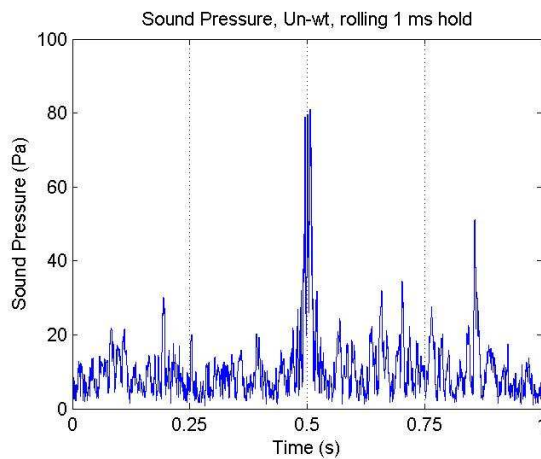
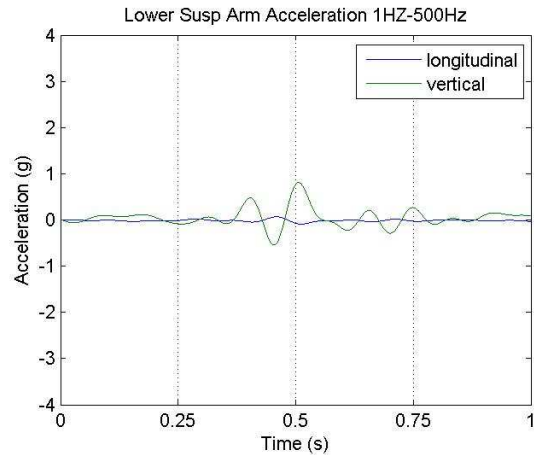
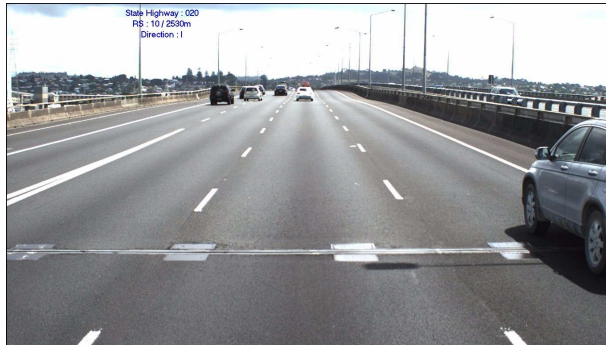
Time: 10-49-32

Marker No: 3

Latitude: -36.934202

Longitude: 174.788553

Speed (km/h): 79.5



Acceleration – Maximum vertical (g): 0.81
Acceleration – Maximum longitudinal (g): 0.09
Displacement – Maximum vertical (mm): 8.24
Displacement – Maximum longitudinal (mm): 0.91
Noise Peak Sound Pressure (Pa): 81.00
Average sound level, Leq(1s) (dB): 114.24
Average sound level, LAeq(1s) (dB): 112.73
Maximum sound level, Leq(125ms) (dB): 118.14
Maximum sound level, LAeq(125ms) (dBA): 120.91

Bridge Structure Number: BSN129

Name: MANGERE BRIDGE (NORTHBOUND)

Route Position: 20 10 2.53

Direction: Increasing

Joint Type: Rubber seals + vert. steel plates

Road Surface Type: Asphalt

Test Details:

Operator: I.Kvatch

Date: 15-05-13

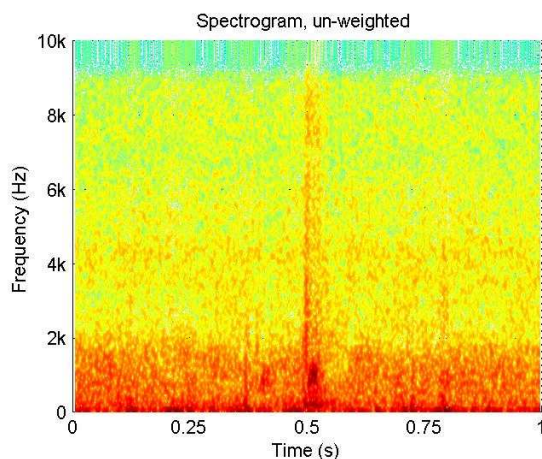
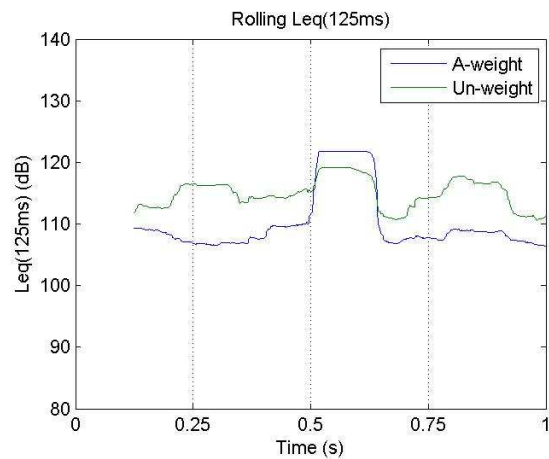
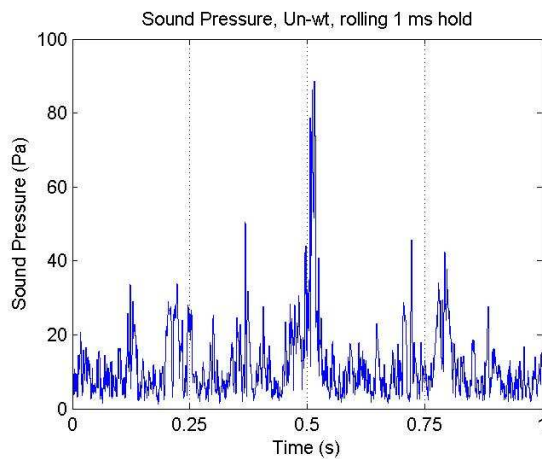
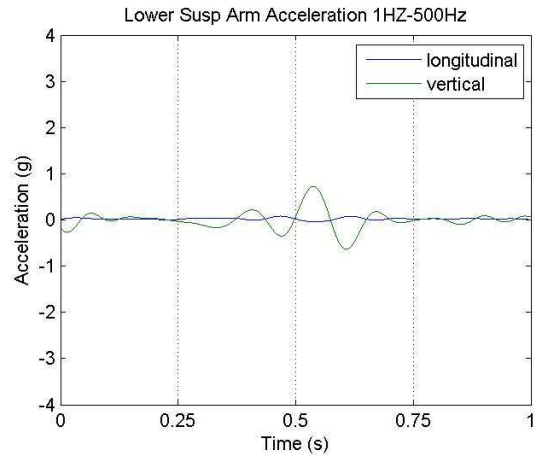
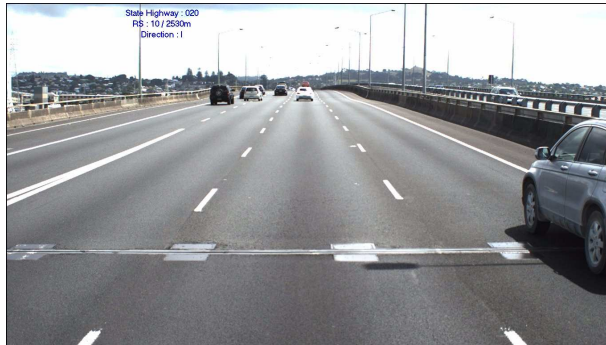
Time: 10-40-21

Marker No: 4

Latitude: -36.932468

Longitude: 174.787600

Speed (km/h): 80.6



Acceleration – Maximum vertical (g): 0.73
Acceleration – Maximum longitudinal (g): 0.08
Displacement – Maximum vertical (mm): 7.39
Displacement – Maximum longitudinal (mm): 0.84
Noise Peak Sound Pressure (Pa): 88.55
Average sound level, Leq(1s) (dB): 115.25
Average sound level, LAeq(1s) (dB): 113.88
Maximum sound level, Leq(125ms) (dB): 119.19
Maximum sound level, LAeq(125ms) (dBA): 121.84

Bridge Structure Number: BSN129

Name: MANGERE BRIDGE (NORTHBOUND)

Route Position: 20 10 2.53

Direction: Increasing

Joint Type: Rubber seals + vert. steel plates

Road Surface Type: Asphalt

Test Details:

Operator: I.Kvatch

Date: 15-05-13

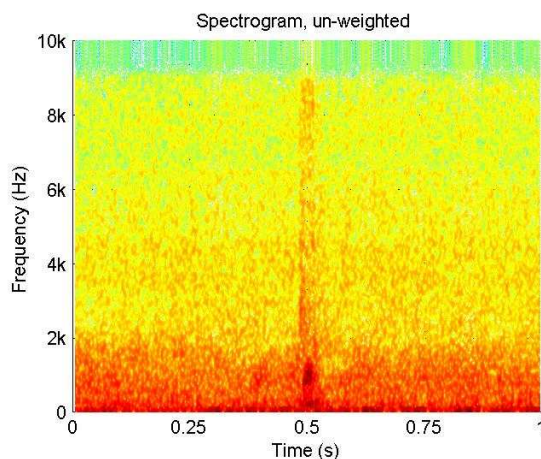
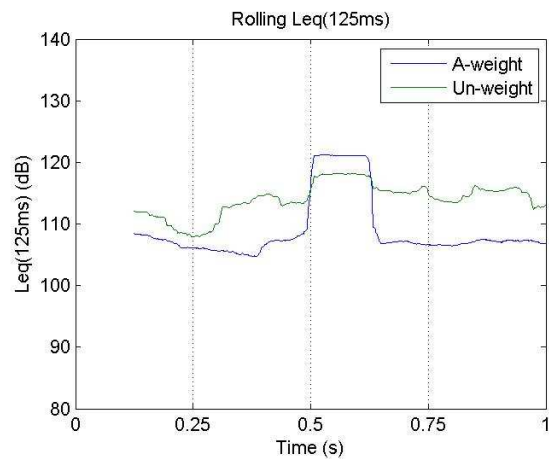
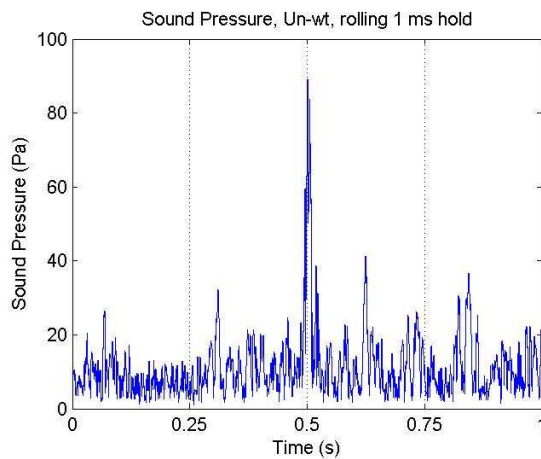
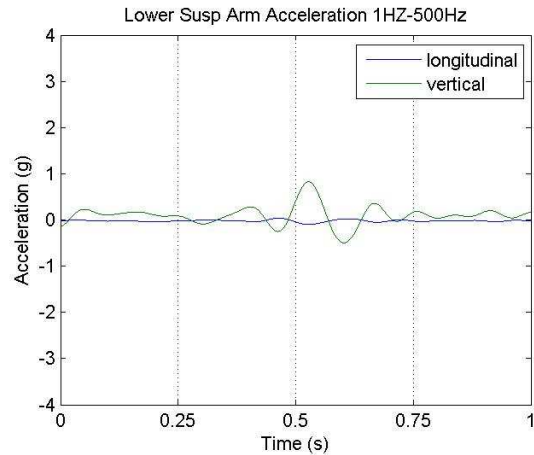
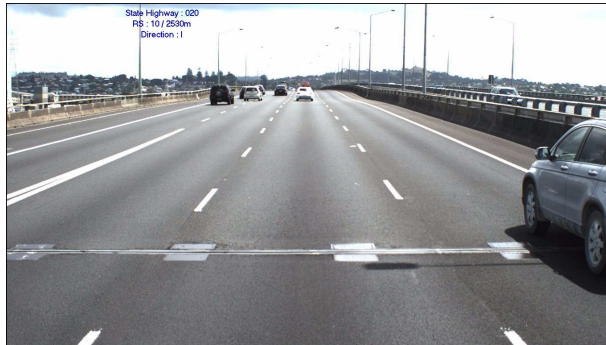
Time: 10-49-32

Marker No: 4

Latitude: -36.932395

Longitude: 174.787545

Speed (km/h): 79.5



Acceleration – Maximum vertical (g): 0.83
Acceleration – Maximum longitudinal (g): 0.09
Displacement – Maximum vertical (mm): 8.40
Displacement – Maximum longitudinal (mm): 0.96
Noise Peak Sound Pressure (Pa): 89.07
Average sound level, Leq(1s) (dB): 114.55
Average sound level, LAeq(1s) (dB): 113.17
Maximum sound level, Leq(125ms) (dB): 118.18
Maximum sound level, LAeq(125ms) (dBA): 121.17

Bridge Structure Number: BSN129

Name: MANGERE BRIDGE (NORTHBOUND)

Route Position: 20 10 2.53

Direction: Increasing

Joint Type: Rubber seals + vert. steel plates

Road Surface Type: Asphalt

Test Details:

Operator: I.Kvatch

Date: 15-05-13

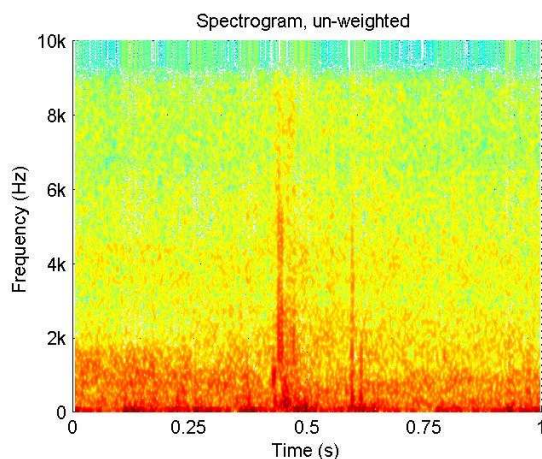
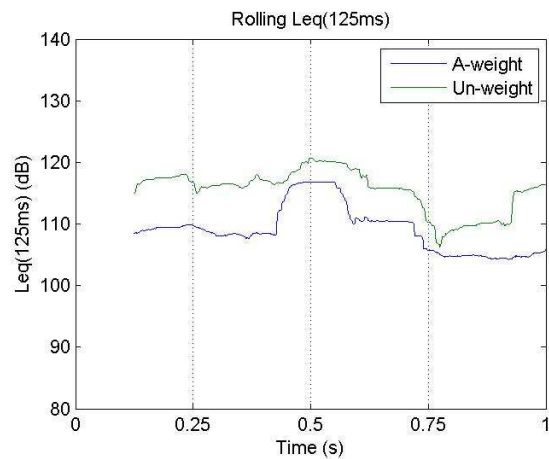
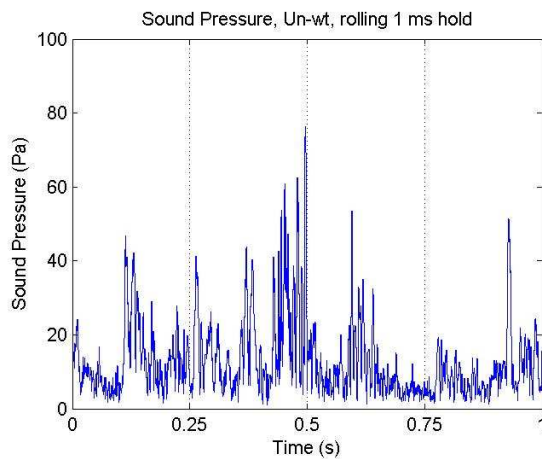
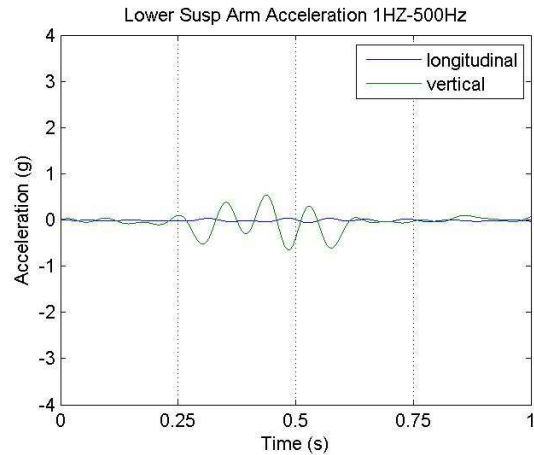
Time: 10-40-21

Marker No: 5

Latitude: -36.931317

Longitude: 174.786565

Speed (km/h): 80.9



Acceleration – Maximum vertical (g): 0.65
Acceleration – Maximum longitudinal (g): 0.06
Displacement – Maximum vertical (mm): 6.55
Displacement – Maximum longitudinal (mm): 0.62
Noise Peak Sound Pressure (Pa): 76.43
Average sound level, Leq(1s) (dB): 116.40
Average sound level, LAeq(1s) (dB): 110.65
Maximum sound level, Leq(125ms) (dB): 120.67
Maximum sound level, LAeq(125ms) (dBA): 116.84

Bridge Structure Number: BSN129

Name: MANGERE BRIDGE (NORTHBOUND)

Route Position: 20 10 2.53

Direction: Increasing

Joint Type: Rubber seals + vert. steel plates

Road Surface Type: Asphalt

Test Details:

Operator: I.Kvatch

Date: 15-05-13

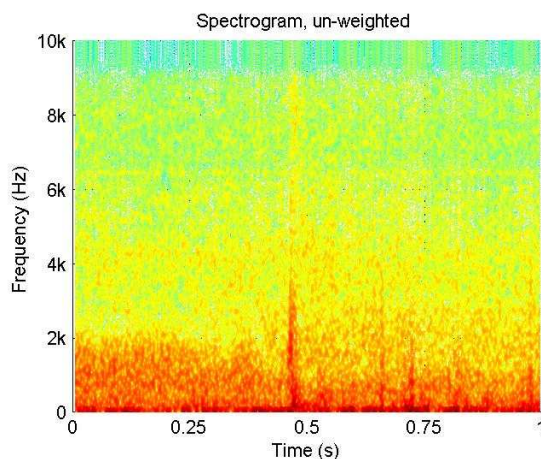
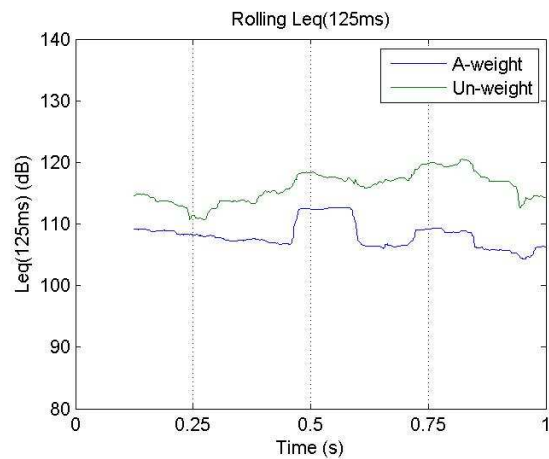
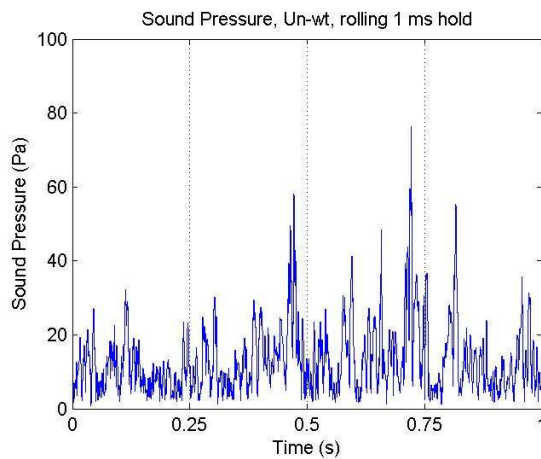
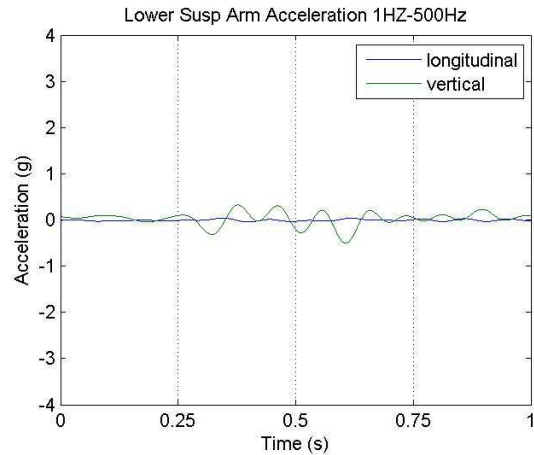
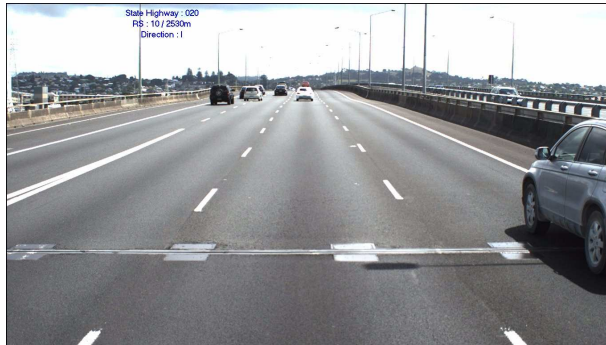
Time: 10-49-32

Marker No: 5

Latitude: -36.931422

Longitude: 174.786675

Speed (km/h): 79.8



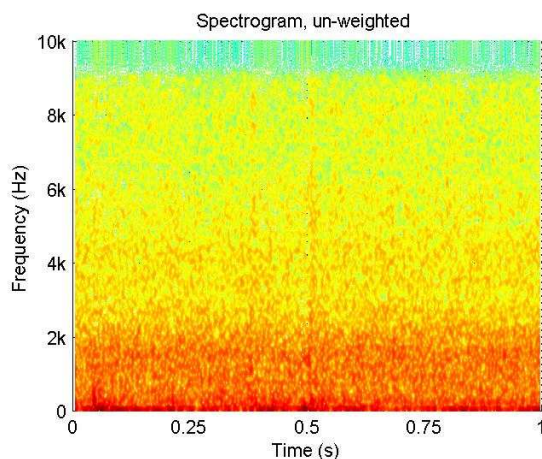
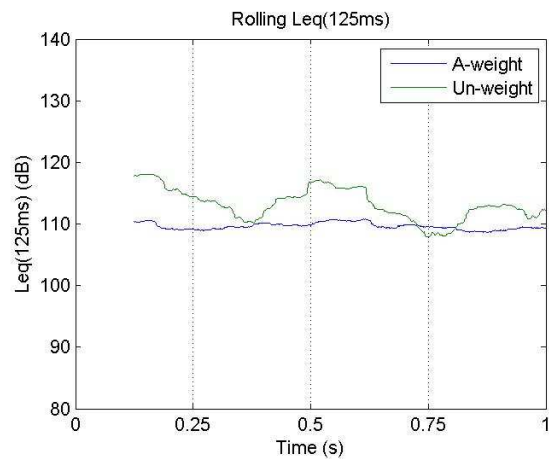
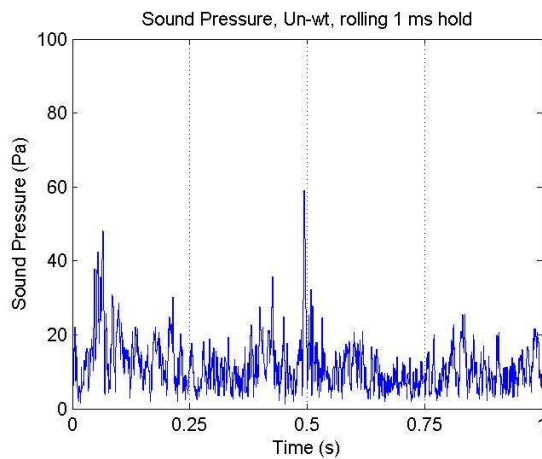
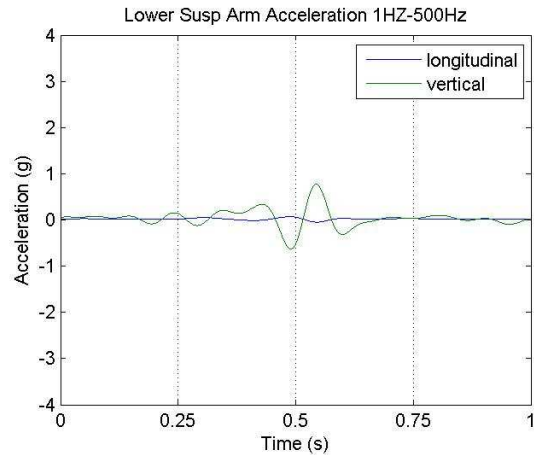
Acceleration – Maximum vertical (g): 0.50
Acceleration – Maximum longitudinal (g): 0.04
Displacement – Maximum vertical (mm): 5.08
Displacement – Maximum longitudinal (mm): 0.45
Noise Peak Sound Pressure (Pa): 76.36
Average sound level, Leq(1s) (dB): 116.42
Average sound level, LAeq(1s) (dB): 108.67
Maximum sound level, Leq(125ms) (dB): 120.47
Maximum sound level, LAeq(125ms) (dBA): 112.67

Bridge Structure Number: BSN136

Name: LINCOLN BRIDGE NO1
Route Position: 16 7 6.57
Direction: Increasing
Joint Type: Steel sliding plate
Road Surface Type: Asphalt

Test Details:

Operator: I.Kvatch Latitude: -36.854498
Date: 15-05-13 Longitude: 174.644078
Time: 11-16-28 Speed (km/h): 79.3
Marker No: 1



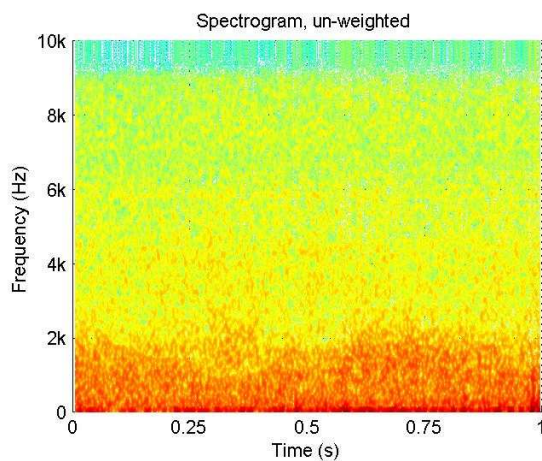
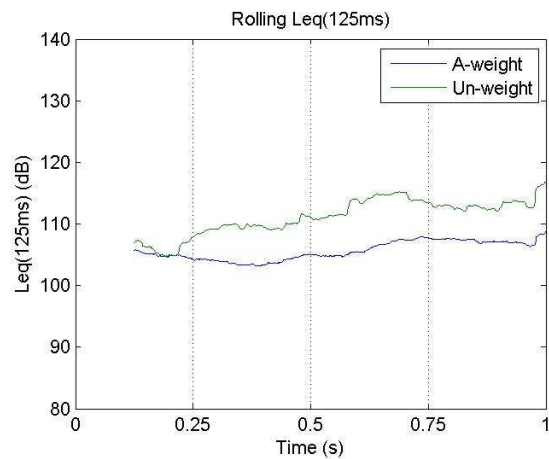
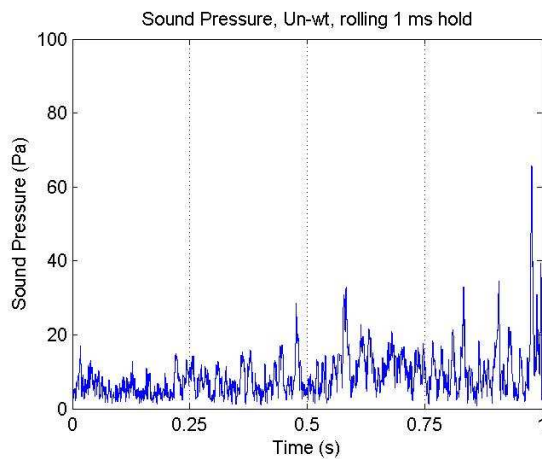
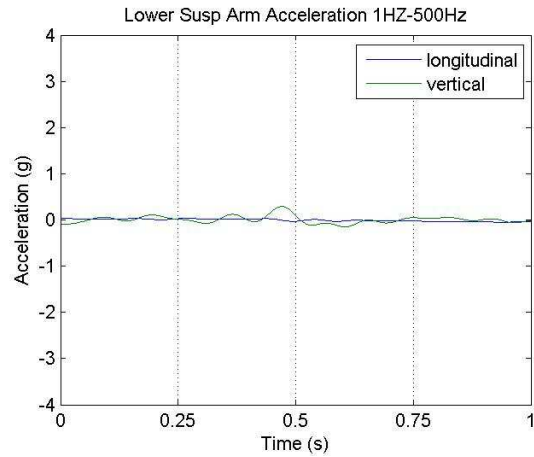
Acceleration – Maximum vertical (g): 0.78
Acceleration – Maximum longitudinal (g): 0.07
Displacement – Maximum vertical (mm): 7.92
Displacement – Maximum longitudinal (mm): 0.71
Noise Peak Sound Pressure (Pa): 59.00
Average sound level, Leq(1s) (dB): 114.24
Average sound level, LAeq(1s) (dB): 109.68
Maximum sound level, Leq(125ms) (dB): 118.10
Maximum sound level, LAeq(125ms) (dBA): 110.83

Bridge Structure Number: BSN136

Name: LINCOLN BRIDGE NO1
Route Position: 16 7 6.57
Direction: Increasing
Joint Type: Steel sliding plate
Road Surface Type: Asphalt

Test Details:

Operator: I.Kvatch Latitude: -36.846695
Date: 15-05-13 Longitude: 174.629518
Time: 11:35:24 Speed (km/h): 71.3
Marker No: 1



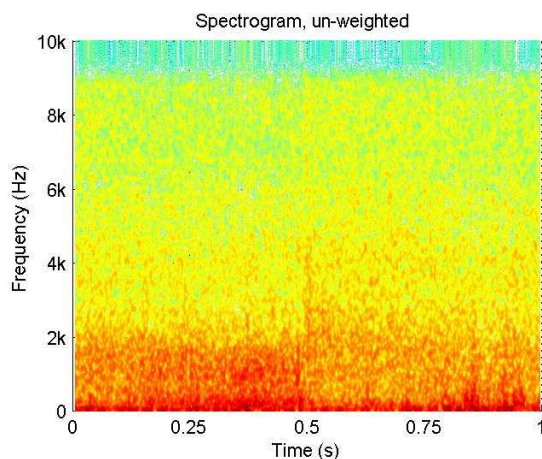
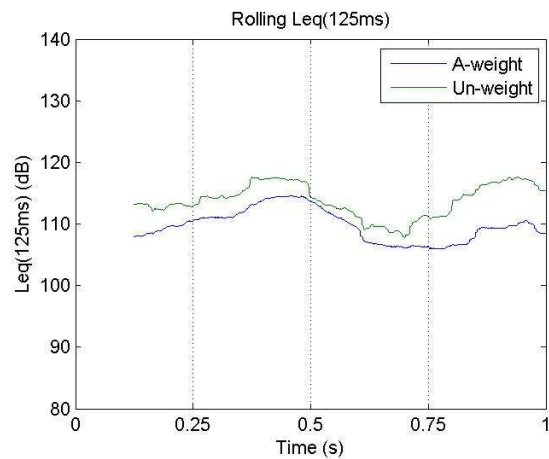
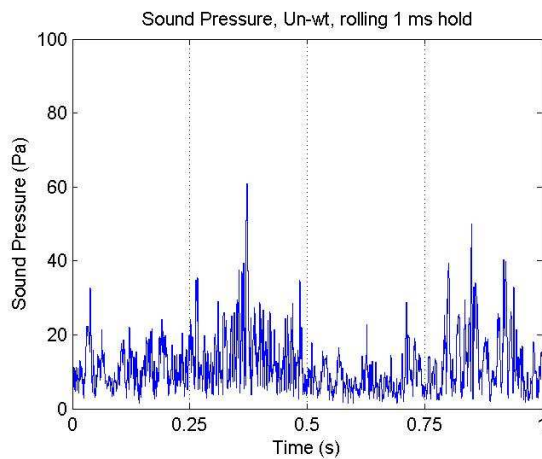
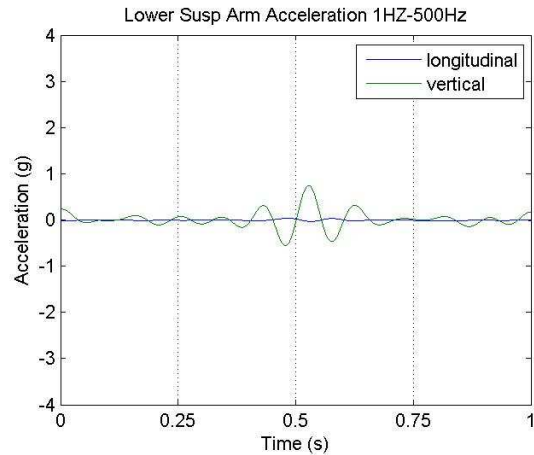
Acceleration – Maximum vertical (g): 0.29
Acceleration – Maximum longitudinal (g): 0.05
Displacement – Maximum vertical (mm): 2.96
Displacement – Maximum longitudinal (mm): 0.53
Noise Peak Sound Pressure (Pa): 65.79
Average sound level, Leq(1s) (dB): 112.64
Average sound level, LAeq(1s) (dB): 106.30
Maximum sound level, Leq(125ms) (dB): 116.82
Maximum sound level, LAeq(125ms) (dBA): 108.71

Bridge Structure Number: BSN136

Name: LINCOLN BRIDGE NO1
Route Position: 16 7 6.57
Direction: Increasing
Joint Type: Steel sliding plate
Road Surface Type: Asphalt

Test Details:

Operator: I.Kvatch Latitude: -36.854050
Date: 15-05-13 Longitude: 174.643258
Time: 11-16-28 Speed (km/h): 79.5
Marker No: 2



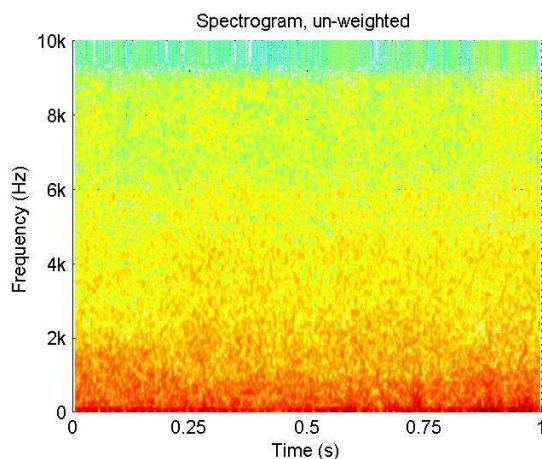
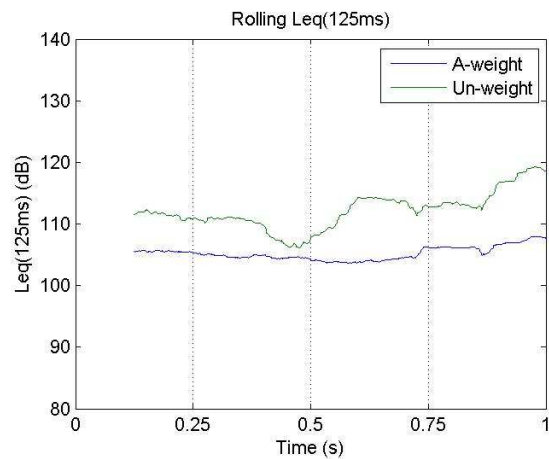
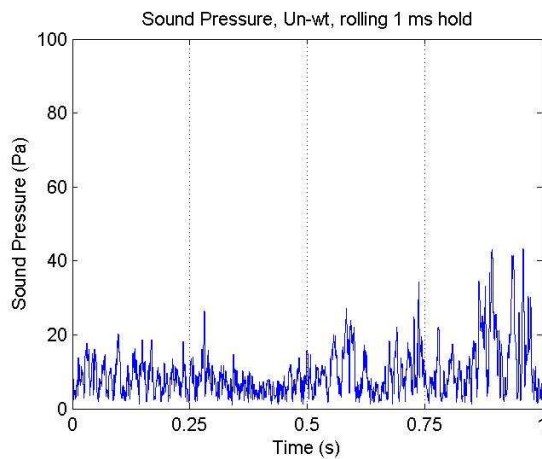
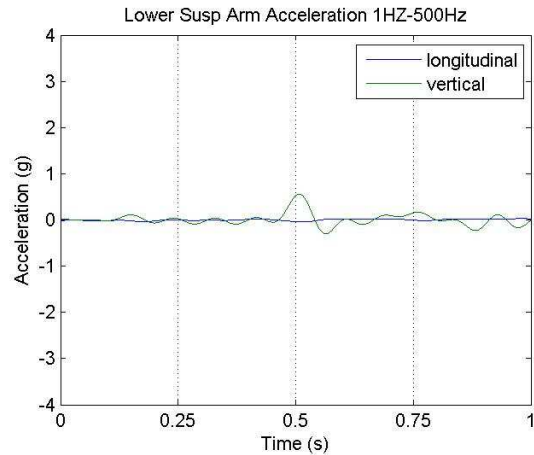
Acceleration – Maximum vertical (g): 0.75
Acceleration – Maximum longitudinal (g): 0.04
Displacement – Maximum vertical (mm): 7.57
Displacement – Maximum longitudinal (mm): 0.36
Noise Peak Sound Pressure (Pa): 60.86
Average sound level, Leq(1s) (dB): 114.50
Average sound level, LAeq(1s) (dB): 110.31
Maximum sound level, Leq(125ms) (dB): 117.68
Maximum sound level, LAeq(125ms) (dBA): 114.56

Bridge Structure Number: BSN136

Name: LINCOLN BRIDGE NO1
Route Position: 16 7 6.57
Direction: Increasing
Joint Type: Steel sliding plate
Road Surface Type: Asphalt

Test Details:

Operator: I.Kvatch Latitude: -36.846452
Date: 15-05-13 Longitude: 174.629185
Time: 11:35:24 Speed (km/h): 72.4
Marker No: 2



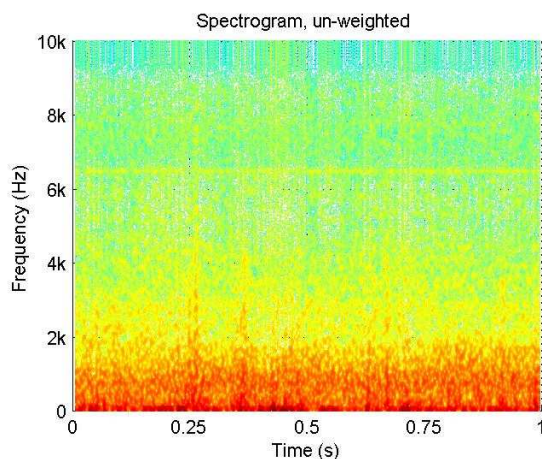
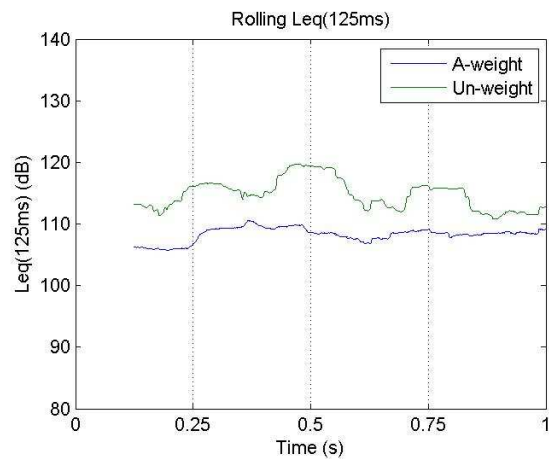
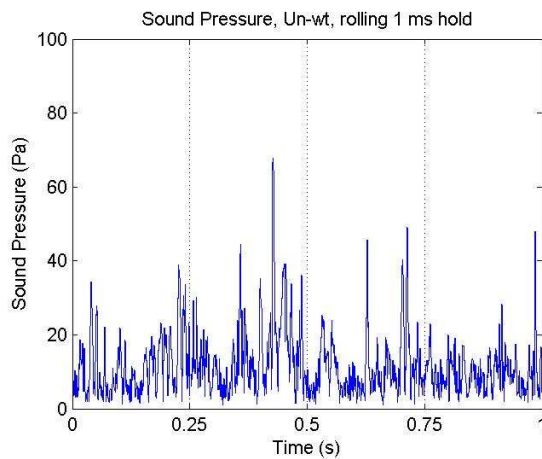
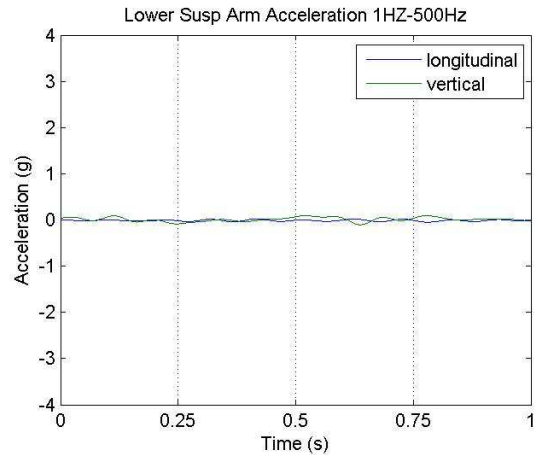
Acceleration – Maximum vertical (g): 0.56
Acceleration – Maximum longitudinal (g): 0.04
Displacement – Maximum vertical (mm): 5.70
Displacement – Maximum longitudinal (mm): 0.44
Noise Peak Sound Pressure (Pa): 43.34
Average sound level, Leq(1s) (dB): 113.67
Average sound level, LAeq(1s) (dB): 105.54
Maximum sound level, Leq(125ms) (dB): 119.28
Maximum sound level, LAeq(125ms) (dBA): 108.01

Bridge Structure Number: BSN137

Name: LINCOLN BRIDGE NO2
Route Position: 16 7 6.57
Direction: Decreasing
Joint Type: Rubber seal (solid)
Road Surface Type: Asphalt

Test Details:

Operator: I.Kvatch Latitude: -36.851173
Date: 15-05-13 Longitude: 174.637813
Time: 11-19-40 Speed (km/h): 78.9
Marker No: 1



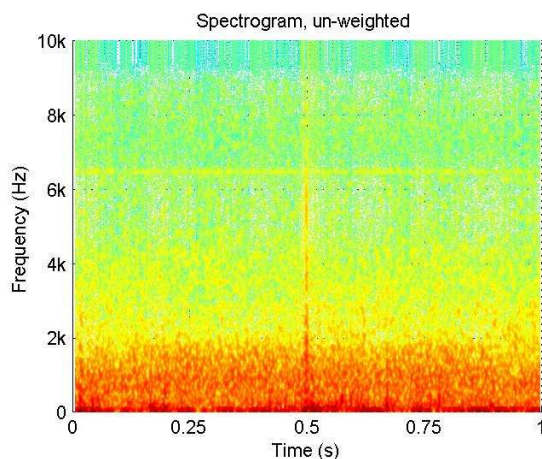
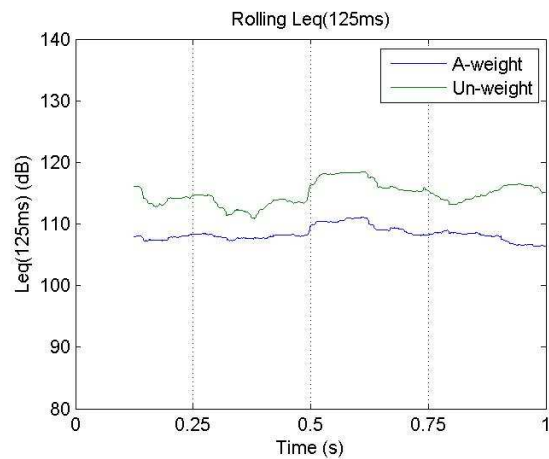
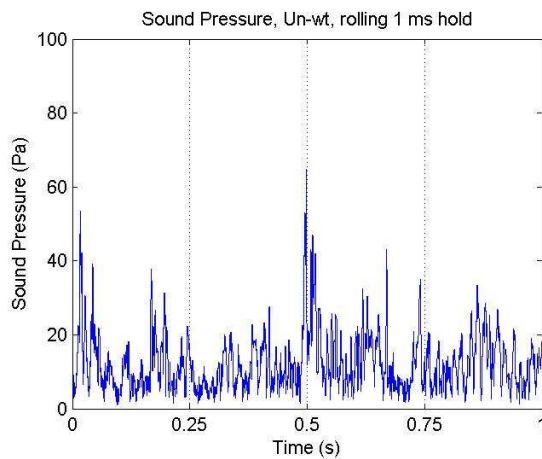
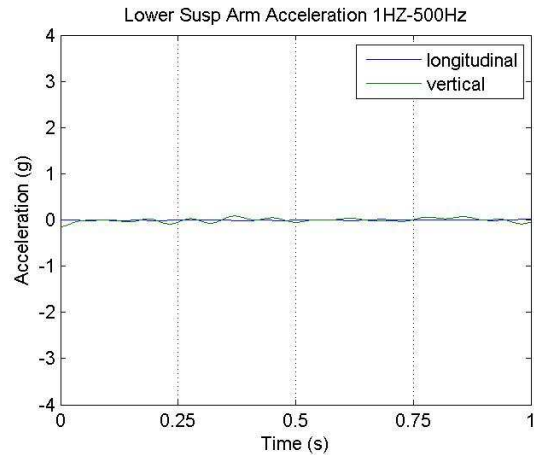
Acceleration – Maximum vertical (g): 0.10
Acceleration – Maximum longitudinal (g): 0.05
Displacement – Maximum vertical (mm): 1.06
Displacement – Maximum longitudinal (mm): 0.50
Noise Peak Sound Pressure (Pa): 67.80
Average sound level, Leq(1s) (dB): 115.29
Average sound level, LAeq(1s) (dB): 108.41
Maximum sound level, Leq(125ms) (dB): 119.70
Maximum sound level, LAeq(125ms) (dBA): 110.56

Bridge Structure Number: BSN137

Name: LINCOLN BRIDGE NO2
Route Position: 16 7 6.57
Direction: Decreasing
Joint Type: Rubber seal (solid)
Road Surface Type: Asphalt

Test Details:

Operator: I.Kvatch Latitude: -36.851495
Date: 15-05-13 Longitude: 174.638448
Time: 11-19-40 Speed (km/h): 80.4
Marker No: 2



Acceleration – Maximum vertical (g): 0.14
Acceleration – Maximum longitudinal (g): 0.02
Displacement – Maximum vertical (mm): 1.46
Displacement – Maximum longitudinal (mm): 0.17
Noise Peak Sound Pressure (Pa): 64.72
Average sound level, Leq(1s) (dB): 115.40
Average sound level, LAeq(1s) (dB): 108.42
Maximum sound level, Leq(125ms) (dB): 118.48
Maximum sound level, LAeq(125ms) (dBA): 111.08