

SECTION 5

TEMPORARY WARNING SIGNS

Interim Update : January 2010

Note: This section of MOTSAM is expected to be superseded during 2010 when it is merged with COPTTM and published as Part 8 of the Traffic Control Devices Manual.

CONTENTS

		Page Number	Page Date
SECTION 5: TEMPORARY WARNING SIGNS (Design, Policy And Location)			
FIGURE 5.1	TEMPORARY WARNING SIGNS – STANDARD TW – A: DIAMOND SIGN PLATE	5-1	Jan 2010
FIGURE 5.2	TEMPORARY WARNING SIGNS – STANDARD TW – B: SQUARE SIGN PLATE	5-2	"
FIGURE 5.3	TEMPORARY WARNING SIGNS STANDARD TW SIGN SUPPLEMENTARY PLATES	5-3	"
FIGURE 5.4	TEMPORARY WARNING SIGNS STANDARD TW SIGN SUPPLEMENTARY PLATES	5-4	"
TW-1	ROAD WORKS	5-5	"
TW-1B.1	ROADWORKS – 1 or 2 km	5-6	"
TW-1B.2	ROAD WORKS – 1 or 2 km DELAYS POSSIBLE	5-7	"
TW-1B.3	ROAD WORKS – "___" 0 km/h AHEAD	5-8	"
TW-1.1	ROAD WORKS – NEXT "___" km	5-9	"
TW-1.2	ROAD WORKS – NEW SEAL/WET TAR	5-10	"
TW-1.3	ROAD WORKS – SPECIALIST MOBILE PLANT	5-12	"
TW-1.4	ROAD WORKS – ROAD MARKING	5-14	"
TW-1.5	ROAD WORKS – ON SIDE ROAD	5-15	"
TW-1.6	ROAD WORKS – SHOULDER CLOSED	5-16	"
TW-1.7	ROAD WORKS – SURVEYING	5-17	"
TW-1.8	ROAD WORKS – BRIDGE REPAIRS	5-18	"
TW-2	OTHER HAZARD (never erected separately)	5-19	"
TW-2.1	OTHER HAZARD – FLOODING	5-20	"
TW-2.2	OTHER HAZARD – WASHOUT	5-21	"
TW-2.3	OTHER HAZARD – LINEMEN	5-22	"
TW-2.4	OTHER HAZARD – BLASTING	5-23	"
TW-2.5	OTHER HAZARD – TREE FELLING	5-24	"
TW-2.6	OTHER HAZARD – LOGGING TRUCKS	5-25	"
TW-2.7	OTHER HAZARD – TRUCKS CROSSING	5-26	"
TW-2.8	OTHER HAZARD – NO ROAD MARKING	5-27	"
TW-2.9	OTHER HAZARD – SIGNALS CHANGED	5-28	"
TW-2.10	OTHER HAZARD – SIGNALS NOT WORKING	5-29	"
TW-2.11	OTHER HAZARD – NEW ROAD LAYOUT	5-30	"
TW-2.12	OTHER HAZARD – TRAFFIC SURVEY	5-31	"
TW-2.13	OTHER HAZARD – VULNERABLE ROAD USER EVENT	5-32	"
TW-2.14	OTHER HAZARD – ACCIDENT	5-34	"
TW-2.15	OTHER HAZARD – FIRE	5-35	"
TW-2.16	OTHER HAZARD – VULNERABLE ROAD USERS AHEAD	5-36	"
TW-2.17	OTHER HAZARD – FUNERAL	5-38	"
TW-2.18	OTHER HAZARD – FILM CREW	5-39	"
TW-3	SLIPS	5-40	"

**NOTE: THE PAGE NUMBERS IN RED ARE DIFFERENT TO THOSE
IN THE 1998 HARD COPY ISSUE.**

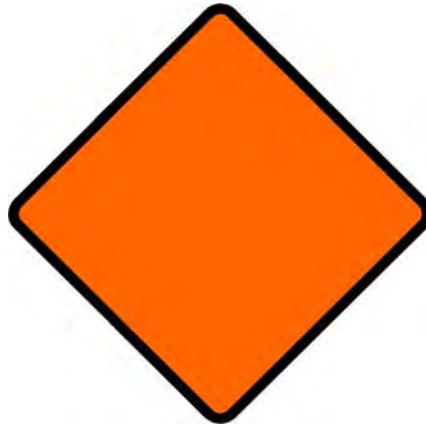
TEMPORARY WARNING SIGNS

Part 1 Signs

Jan 2010

TW-4	SLIPPERY SURFACE	5-41	Jan 2010
TW-4.1	SLIPPERY SURFACE – ICE / GRIT	5-42	"
TW-5	GRAVEL SURFACE	5-43	"
TW-5.1	GRAVEL SURFACE – NEW SEAL	5-44	"
TW-5.2	GRAVEL SURFACE – SEAL REPAIRS	5-45	"
TW-6	STOCK – TEMPORARY (CATTLE)	5-46	"
TW-6.1	STOCK – TEMPORARY (SHEEP)	5-46	"
TW-7	LANE CLOSED – TWO LANE ONE WAY ROAD	5-47	"
TW-7.1	LANE CLOSED – THREE LANE ONE WAY ROAD	5-48	"
TW-7.1.1	CENTRE LANE CLOSED – THREE LANE ONE WAY ROAD	5-49	"
TW-7.2	LANE CLOSED -- FOUR LANE ONE WAY ROAD	5-50	"
TW-8	LANE SHIFT – TWO LANE ONE WAY ROAD	5-51	"
TW-8.1	LANE SHIFT – THREE LANE ONE WAY ROAD	5-52	"
TW-9	MERGING TRAFFIC – MAIN ROAD	5-53	"
TW-10	MERGING TRAFFIC – SIDE ROAD	5-54	"
TW-11	ADVANCE EXIT	5-55	"
TW-12	EXIT DIRECTION	5-56	"
TW-13/13.1	ONE LANE – LEFT/RIGHT/BOTH SIDE NARROWING	5-57	"
TW-14	TRAFFIC SIGNALS – TEMPORARY	5-58	"
TW-15	MANUAL TRAFFIC CONTROL	5-59	"
TW-15.1	MANUAL TRAFFIC CONTROL (with supplementary TW-18 plate)	5-60	"
TW-16	WORKS END (used with TW-1 ROAD WORKS signs)	5-61	"
TW-17	THANK YOU (used with TW-2, TW-3, TW-4, TW-5 AND TW-6 signs)	5-62	"
TW-18	PLEASE STOP ON REQUEST	5-63	"
TW-19	DRY YOUR BRAKES	5-64	"
TW-20	ROAD CLOSED AHEAD	5-65	"
TW-21	DETOUR AHEAD – FOLLOW " <i>symbol</i> "	5-66	"
TW-22	DETOUR – DIRECTION INDICATOR	5-67	"
TW-23	DETOUR ENDS	5-68	"
TW-24	CEMENT SPLASHES	5-69	"
TW-24.1	LIME SPLASHES	5-69	"
TW-25	PILOT CAR FOLLOW ME (vehicle mounted)	5-70	"
TW-26	ROAD WORKS (vehicle mounted)	5-71	"
TW-27	ROAD INSPECTON (vehicle mounted)	5-72	"
TW-28	SITE ACCESS – " __ "00m	5-73	"
TW-29	SITE ACCESS – DIRECTION INDICATOR	5-74	"
TW-30	CROSSING CLOSED - PLEASE USE ALTERNATIVE CROSSING	5-75	"
TW-31	FOOTPATH CLOSED – PLEASE USE OTHER SIDE	5-76	"
TW-32	TEMPORARY PEDESTRIAN/CYCLE ROUTE	5-77	"
TW-33	STOP / GO PADDLE	5-79	"
TW-34	PASS WITH CARE (vehicle mounted)	5-80	"
TW-35	DIVERGE	5-81	"

**NOTE: THE PAGE NUMBERS IN RED ARE DIFFERENT TO THOSE
IN THE 1998 HARD COPY ISSUE.**



SYMBOL *	: black (unless specified otherwise)
BACKGROUND	: TW - 1A and TW - 2A signs: Reflectorised fluorescent orange. All other TW - A signs: Class 1 reflectorised orange (unless specified otherwise)
BORDER	: black

For dimensions refer to <http://www.nzta.govt.nz/resources/traffic-control-devices-manual/sign-specifications/>

GENERAL SPECIFICATION: TW - A Signs

The main component of most TW - A Temporary Warning Signs is a diamond shaped sign plate, square with one diagonal vertical and of minimum size 750 mm x 750 mm, displaying an approved symbol. An approved word legend may be displayed on a rectangular T W / A or B supplementary plate mounted up to 100 mm below the main diamond sign, to form a sign combination.

TW - 7A and TW - 8A sign combinations have a square shaped sign plate of minimum size 750 mm x 750 mm displaying an approved symbol and a rectangular TW / C supplementary plate displaying a distance legend mounted immediately below the main square sign.

No other symbols or word legends may be used unless they have been approved by the NZ Transport Agency.

All sign border corners are rounded. It is preferable that sign plate corners are also rounded, to match the border.

Signs should be fitted to white painted, unpainted galvanised steel or unpainted aluminium stands which will provide stability under weather conditions likely to be experienced on all but very rare occasions with the centre of the diamond sign being located at least 1.25 m above the road surface. Portable sign stands should conform to the requirements of NZS 5414 : 1977 SPECIFICATION FOR THE CONSTRUCTION OF TRAFFIC SIGNS.

Note that with the exception of sandwich board mounted RP - 1.1 NO STOPPING AT ALL TIMES signs used in temporary road works situations, stand mounted sign plates should be oriented normal to the road surface. Two A type Temporary Warning Signs, TW - 25 A (25.1A) CEMENT (LIME) SPLASHES and TW - 17A TRAFFIC SURVEY SUPPLEMENTARY - PLEASE STOP ON REQUEST consist only of word legends on rectangular sign plates. These signs are additional traffic direction signs which are mounted on separate stands just beyond the primary TW - A advance warning sign combination, ie. TW - 1A ROAD WORKS and TW - 2.15A OTHER HAZARD - TRAFFIC SURVEY respectively.

The above signs together with TW - 7A LANE CLOSED - ONE WAY ROAD, TW - 8A LANE SHIFT - ONE WAY ROAD, TW - 15A WORKS END, TW - 16A THANK YOU, TW - 18A DRY YOUR BRAKES, TW - 19A ROAD CLOSED AHEAD, TW - 20A DETOUR AHEAD - FOLLOW SYMBOL, TW - 22A DETOUR - DIRECTION INDICATOR and TW - 23A DETOUR ENDS signs are the only rectangular A type TW signs that may be erected separately.

The TW - 34 STOP/GO PADDLE sign consists of a double sided disc bearing specific word legends.



SYMBOL *	:	<i>black (unless otherwise specified)</i>
BACKGROUND	:	<i>Class 1 reflectorised white</i>
DIAMOND	:	<i>reflectorised fluorescent orange</i>
BORDERS	:	<i>black</i>

For dimensions refer to <http://www.nzta.govt.nz/resources/traffic-control-devices-manual/sign-specifications/>

GENERAL SPECIFICATION: TW - B Signs

The main component of most TW - B Temporary Warning Signs is a square shaped sign plate of minimum size 1200 mm x 1200 mm. Superimposed on the sign plate is a diamond shaped sign, square with one diagonal vertical and of minimum size 850 mm x 850 mm, displaying an approved symbol.

An approved word legend may be displayed on a rectangular TW / D or E supplementary plate mounted immediately below the main square sign, to form a sign combination.

No other symbols or word legends may be used unless they have been approved by the NZ Transport Agency.

All sign border corners are rounded. It is preferable that sign plate corners are also rounded, to match the border.

Signs should be fitted to white painted, unpainted galvanised steel or unpainted aluminium stands which will ensure stability under weather conditions likely to be experienced on all but very rare occasions with the underside of the sign, or the supplementary plate of a sign combination, being located at least 1.0 m above the road surface. Portable sign stands should conform to the requirements of NZS 5414 : 1977 SPECIFICATION FOR THE CONSTRUCTION OF TRAFFIC SIGNS.

Note that with the exception of sandwich board mounted RP - 1.1 NO STOPPING AT ALL TIMES signs (used in the temporary road works situation) stand mounted sign plates should be oriented normal to the road surface.

Two B type Temporary Warning Signs, TW - 25B (25.1B) CEMENT (LIME) SPLASHES and TW - 17B TRAFFIC SURVEY SUPPLEMENTARY - PLEASE STOP ON REQUEST consist only of word legends on rectangular sign plates. These signs are additional traffic direction signs and are mounted on separate stands just beyond the primary TW - B type advance warning sign combination, i.e. TW - 1B ROAD WORKS and TW - 2.15B OTHER HAZARD - TRAFFIC SURVEY respectively.

The above signs together with TW - 7B LANE CLOSED - ONE WAY ROAD, TW - 8B LANE SHIFT - ONE WAY ROAD, TW - 9B MERGING TRAFFIC - Main ROAD, TW - 10B MERGING TRAFFIC - SIDE ROAD, TW - 15 B WORKS END, TW - 16B THANK YOU, TW - 18B DRY YOUR BRAKES, TW - 19B ROAD CLOSED AHEAD, TW - 21B DETOUR AHEAD - FOLLOW SYMBOL, TW - 22B DETOUR - DIRECTION INDICATOR, TW - 23B DETOUR ENDS and TW - 24 EXIT DIRECTION signs are the only B type TW signs that may be erected separately.

The TW - 34 STOP/GO PADDLE sign consists of a double sided disc bearing specific word legends.

FIGURE 5.2 **TEMPORARY WARNING SIGNS**
STANDARD TW - B SQUARE SIGN PLATE



LEGEND * : black
 BACKGROUND : TW - 1A and TW - 2A signs: Reflectorised fluorescent orange
 All other TW - A signs: Class 1 reflectorised orange (unless specified otherwise)
 BORDER : black

TYPE TW / A



LEGEND * : black
 BACKGROUND : TW - 1A and TW - 2A signs: Reflectorised fluorescent orange
 All other TW - A signs: Class 1 reflectorised orange (unless specified otherwise)
 BORDER : black

* Refer to individual sign specifications for details of approved word legends.

TYPE TW / B

For dimensions refer to <http://www.nzta.govt.nz/resources/traffic-control-devices-manual/sign-specifications/>

GENERAL SPECIFICATION

The supplementary sign component of an A type Temporary Warning Sign combination is a rectangular shaped TW / A or TW / B sign plate of type, as detailed above, upon which is displayed an approved word legend.

Approved word legends are detailed for individual sign combinations. No other legend may be used unless it has been approved by the NZ Transport Agency.

Sign border corners are rounded. It is preferable that sign plate corners are also rounded, to match the border.



LEGEND * : *black*
BACKGROUND : *reflectorised fluorescent orange*
BORDER : *black*

* Refer to individual sign specifications for details of approved word legends.

TYPE TW / C



LEGEND * : *black*
BACKGROUND # : *reflectorised fluorescent orange*
BORDER : *black*

* Refer to individual sign specifications for details of approved word legends.

A white reflectorised background is used for the TW - 1B/3 '0 km/h AHEAD supplementary sign plate.

TYPE TW / D

For dimensions refer to <http://www.nzta.govt.nz/resources/traffic-control-devices-manual/sign-specifications/>

GENERAL SPECIFICATION

The supplementary sign component of a B type Temporary Warning Sign combination is a rectangular shaped TW / C or TW / D sign plate, as detailed above, upon which is displayed an approved word legend.

Approved word legends are detailed for individual sign combinations. No other legend may be used unless it has been approved by the NZ Transport Agency.

Sign border corners are rounded. It is preferable that sign plate corners are also rounded, to match the border.

TEMPORARY WARNING SIGNS

FIGURE 5.4 STANDARD TW SIGN SUPPLEMENTARY PLATES

Policy: A TW - 1 sign consist of a ROADWORKS symbol displayed on a standard TW - A or TW - B sign plate.

TW - 1 advance warning signs may only be erected by employees of the road controlling authority , or with permission of the road controlling authority by contractors or other authorised persons, when any type of road work from minor maintenance patching to major earthmoving is actually in progress.

TW - 1 signs may also be erected by employees of a railway operator to facilitate maintenance of level crossing alarms.

The signs should be displayed when men or machines are working on or near the roadway or where the road surface is in a rough state during construction and the length does not exceed 1 km. If the length is more than 1 km a TW - 1.1 ROAD WORKS NEXT " " km signs should be used.

Where appropriate

TW -1.2, TW - 1.3, TW -1.4, TW -1.7, TW - 2.3, TW - 4, TW - 5 or TW - 5.1 signs should be used in preference to the TW - 1 sign.

Signs should be fitted to white painted, plain galvanised steel or plain aluminium stands. Under weather conditions likely to be experienced on all but very rare occasions the stands must be stable when:

- the centre of a TW - 1A diamond sign is located at least 1.25 m above the road surface, or
- the underside of a TW - 1B sign is located at least 1.0 m above the road surface.

Location: Normally, TW - 1 signs should be erected clear of the traffic lanes. Where this is not practicable they shall be located so as to be clearly visible to approaching drivers and not present an obstruction to traffic, cyclists or pedestrians.

The NZTA Code of Practice for Temporary Traffic Management specifies location and layout requirements for Temporary Warning signs. Special attention must be given to ensuring that the minimum clear visibility and spacing distances between the advance sign and the start of the road works site are achieved.



TW - 1A

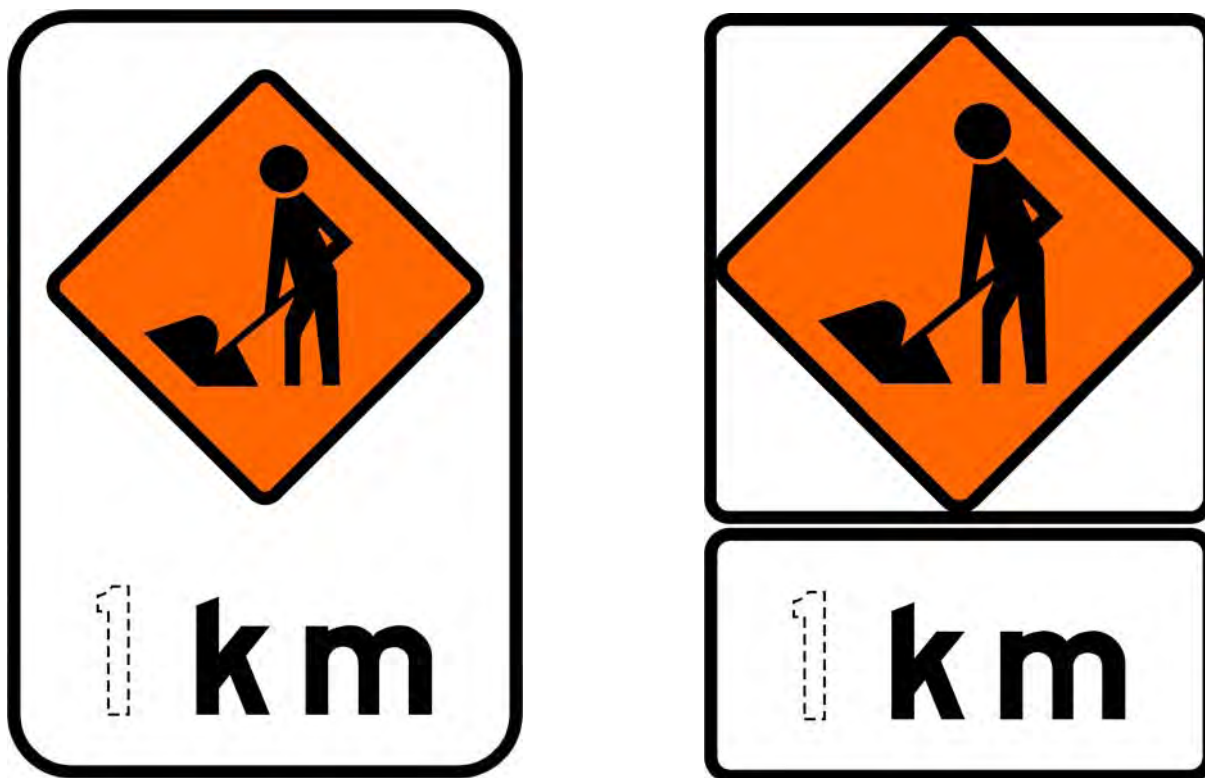
LEGEND	:	<i>black</i>
BACKGROUND	:	<i>reflectorised fluorescent orange</i>
BORDERS	:	<i>black</i>



TW - 1B

LEGEND	:	<i>black</i>
BACKGROUND	:	<i>Class 1 reflectorised white</i>
TW DIAMOND	:	<i>reflectorised fluorescent orange</i>
BORDERS	:	<i>black</i>

For dimensions refer to <http://www.nzta.govt.nz/resources/traffic-control-devices-manual/sign-specifications/>



BACKGROUND : Class 1 reflectorised white
TW - 1A SIGN : reflectorised fluorescent orange
BORDER AND LEGEND : black
NUMERAL : 300 D

For dimensions refer to <http://www.nzta.govt.nz/resources/traffic-control-devices-manual/sign-specifications/>

Policy: A TW -1B.1 sign may be either:

- a single sign displaying a TW - 1A sign and a distance legend "1 km" on a white background, or
- sign combination consisting of a TW - 1A sign and a supplementary sign "1 km".

TW - 1B.1 signs may be used for major long-term temporary traffic management on Level 2 and 3 roads, to give advance warning of road works.

The distance displayed is the distance between the sign and the start of any activity associated with the road works. It should normally be 1 km but if queues form in the vicinity of the sign it shall be moved back a further 1 kilometre and the distance 2 km displayed.

The sign can only be erected by employees of the road controlling authority or, with the permission of the road controlling authority, by contractors or other authorised persons.

The underside of a TW - 1B.1 sign should be located at least 1.0 m above the adjacent road surface.

Location: Normally, TW - 1B.1 signs should be erected clear of the traffic lanes. Where this is not practicable they shall be located so as to be clearly visible to approaching drivers and not present an obstruction to traffic.

The NZTA CODE OF PRACTICE FOR TEMPORARY TRAFFIC MANAGEMENT specifies location and layout requirements for Temporary Warning signs. Special attention must be given to ensuring that the minimum clear visibility and spacing distances between the advance sign and the start of the road works site are achieved.



BACKGROUND	:	<i>Class 1 reflectorised white</i>
TW - 1A SIGN	:	<i>reflectorised fluorescent orange</i>
BORDER AND LEGEND	:	<i>black</i>
NUMERAL	:	<i>300 mm</i>

For dimensions refer to <http://www.nzta.govt.nz/resources/traffic-control-devices-manual/sign-specifications/>

Policy: A TW -1B.2 sign consists of a TW - 1A sign, the distance to the start of the road works and a 'DELAYS POSSIBLE' message displayed on a white backing board.

TW - 1B.2 signs may be used for major long-term temporary traffic management on Level 2 and 3 roads, to give an advance warning of road works where there is a probability of queues forming because of delays caused by the work site activities.

The distance displayed is the distance between the sign and the start of any activity associated with the road works. It should normally be 1 km but if queues form in the vicinity of the sign it shall be moved back a further one kilometre and the distance 2 km displayed.

The sign can only be erected by employees of the road controlling authority or, with the permission of the road controlling authority, by contractors or other authorised persons.

The underside of the sign should be located at least 1.0 m above the adjacent road surface.

Location: Normally, TW - 1B.2 signs should be erected clear of the traffic lanes. Where this is not practicable they shall be located so as to be clearly visible to approaching drivers and not present an obstruction to traffic.

The NZTA CODE OF PRACTICE FOR TEMPORARY TRAFFIC MANAGEMENT specifies location and layout requirements for Temporary Warning signs. Special attention must be given to ensuring that the minimum clear visibility and spacing distances between the advance sign and the start of the road works site are achieved.

SIGN PLATE : TW / D
BACKGROUND : Class 1 reflectorised white
BORDER AND LEGEND : black
NUMERALS : 300 D



TW - 1B.3

For dimensions refer to <http://www.nzta.govt.nz/resources/traffic-control-devices-manual/sign-specifications/>

Policy: A TW -1B.3 sign combination consists of a TW - 1B sign and a supplementary sign “_”0 km/h AHEAD.

TW - 1B.3 signs may be used for temporary traffic management on Level 2 and 3 roads to give advance warning of road works subject to a temporary speed limit restriction. The speed indicated is the temporary speed limit applied at the work site.

The sign can only be erected by employees of the road controlling authority or, with the permission of the road controlling authority, by contractors or other authorised persons.

The underside of the supplementary sign should be located at least 1.0 m above the adjacent road surface.

Location: Normally, TW - 1B.3 signs should be erected on both sides of the road and clear of the traffic lanes. where this is not practicable they shall be located so as to be clearly visible to approaching drivers and not present an obstruction to traffic.

The NZTA CODE OF PRACTICE FOR TEMPORARY TRAFFIC MANAGEMENT specifies location and lay out requirements for Temporary Warning signs. Special attention must be given to ensuring that the minimum clear visibility and spacing distances between the advance sign and the start of the road works site are achieved.



For dimensions refer to

<http://www.nzta.govt.nz/resources/traffic-control-devices-manual/sign-specifications/>



TW - 1.1A *

* reflectorised fluorescent
orange background

Policy: A TW - 1.1 sign combination consists of a TW - 1 ROAD WORKS sign and a supplementary sign NEXT "_ " km, or NEXT "_ "00 m, if the work site is less than one kilometre in length.

TW - 1.1 advance warning signs may only be erected by employees of the controlling authority or with permission of the controlling authority by contractors or other authorised persons, when any type of road work results in a road surface inferior to that on the approaches to the work site and extends for a distance of more than 1 kilometre.

The signs shall be removed when the surface has been improved to a standard equal or better than that on the approaches to the work site. This is the responsibility of the person in charge of the work party at the site.

Signs should be fitted to white painted, plain galvanised steel or plain aluminium stands. Under weather conditions likely to be experienced on all but very rare occasions the stands must be stable when:

- the centre of a TW - 1.1A diamond sign is located at least 1.25 m above the road surface, or
- the underside of a TW - 1.1B sign combination is located at least 1.0 m above the road surface.

Location: Normally, TW - 1.1 signs should be erected clear of the traffic lanes. Where this is not practicable they shall be located so as to be clearly visible to approaching drivers and not present an obstruction to traffic.

The NZTA CODE OF PRACTICE FOR TEMPORARY TRAFFIC MANAGEMENT specifies location and layout requirements for Temporary Warning signs. Special attention must be given to ensuring that the minimum clear visibility and spacing distances between the advance sign and the start of the road works site are achieved.



TW - 1.1B

For dimensions refer to

<http://www.nzta.govt.nz/resources/traffic-control-devices-manual/sign-specifications/>



TW - 1.2.1A *

* reflectorised fluorescent orange background

Policy: A TW - 1.2 sign combination consists of a TW - 1 ROAD WORKS sign and a supplementary sign NEW SEAL or WET TAR.

TW - 1.2 advance warning signs may only be erected by employees of the controlling authority or with permission of the controlling authority by contractors or other authorised persons.

The NEW SEAL supplementary sign should be used to indicate sealing operations and a newly sealed surface while it is susceptible to damage by motor traffic. The signs shall remain in place until the new sealing work has been completed, to give reasonable protection to the sealing, and then must be replaced with TW - 5.1 GRAVEL SURFACE - NEW SEAL signs.

The WET TAR supplementary sign should be used to indicate the subsequent bleeding of a completed seal. It is the responsibility of the person in charge of the work to remove the signs when the bleeding has been repaired.

Signs should be fitted to white painted, plain galvanised steel or plain aluminium stands. Under weather conditions likely to be experienced on all but very rare occasions the stands must be stable when:

- the centre of a TW - 1.2A diamond sign is located at least 1.25 m above the road surface, or
- the underside of a TW - 1.2B sign combination is located at least 1.0 m above the road surface.

Location: Normally, TW - 1.2 signs should be erected clear of the traffic lanes. Where this is not practicable they shall be located so as to be clearly visible to approaching drivers and not present an obstruction to traffic.

The NZTA CODE OF PRACTICE FOR TEMPORARY TRAFFIC MANAGEMENT specifies location and layout requirements for Temporary Warning signs. Special attention must be given to ensuring that the minimum clear visibility and spacing distances between the advance sign and the start of the road works site are achieved.



TW - 1.2.1B



TW - 1.2.2A *

** reflectorised fluorescent orange background*



TW - 1.2.2B

For dimensions refer to <http://www.nzta.govt.nz/resources/traffic-control-devices-manual/sign-specifications/>

Policy: A TW - 1.3 sign combination consists of a W - 1 ROAD WORKS sign and a supplementary sign displaying the name of a specific type of specialist mobile maintenance plant, eg. GRADER, MOWER, PAVEMENT BURNER, ROAD SWEEPER, SKID TESTING, WEED SPRAYER.

TW - 1.3 advance warning signs may only be erected by employees of the controlling authority, or with permission of the controlling authority by contractors or other persons, whenever such maintenance plant not otherwise covered by TW - 1 ROAD WORKS or TW - 1.4 ROADMARKING signs, is operating on the roadway. TW - 1.3 signs shall not be used for such mobile operations as patching where men are working on the road.

Where the maintenance operation is essentially restricted to the shoulder or verge of the road and no obstruction occurs to the traffic lanes other than by the presence of the vehicle itself, a single TW - 1.3 sign may be erected to warn drivers approaching on the affected side of the road. Where a traffic lane is obstructed, TW - 1.3 signs must be placed on each approach before maintenance operations commence.

Where necessary, eg. on winding sections of road, an additional supplementary message NEXT " _ " km, details as for the TW - 1.1 supplementary NEXT " - " km sign, may be displayed in conjunction with the main sign combination. A single line plant item name may be combined with the NEXT " - _ " km message on a two line supplementary sign plate. A separate additional supplementary sign should be used where the plant item is already displayed on a two line sign plate. ***In no cases should the work zone exceed 4 km in length.***

The plant operator must ensure that the TW - 1.3 signs are moved as soon as operations are completed. When not in use the signs should normally be stored on the plant item concerned.

Signs should be fitted to white painted, plain galvanised steel or plain aluminium stands. Under weather conditions likely to be experienced on all but very rare occasions the stands must be stable when:

- the centre of a TW - 1.3A diamond sign is located at least 1.25 m above the road surface, or
- the underside of a TW - 1.3B sign combination is located at least 1.0 m above the road surface.

Location: Normally, TW - 1.3 signs should be erected clear of the traffic lanes. Where this is not practicable they shall be located so as to be clearly visible to approaching drivers and not present an obstruction to traffic.

The NZTA CODE OF PRACTICE FOR TEMPORARY TRAFFIC MANAGEMENT specifies location and layout requirements for Temporary Warning signs. Special attention must be given to ensuring that the minimum clear visibility and spacing distances between the advance sign and the start of the road works site are achieved.



GRADER



**SKID
TESTING**

* reflectorised fluorescent
orange background

TW - 1.3A *

For dimensions refer to <http://www.nzta.govt.nz/resources/traffic-control-devices-manual/sign-specifications/>



MOWER



**WEED
SPRAYER**

TW - 1.3B

For dimensions refer to

<http://www.nzta.govt.nz/resources/traffic-control-devices-manual/sign-specifications/>



TW - 1.4A *

* reflectorised fluorescent orange background

Policy: A TW - 1.4 sign combination consists of a TW - 1 ROAD WORKS sign and a supplementary sign ROAD MARKING.

TW - 1.4 advance warning signs may only be erected by employees of the controlling authority or with permission of the controlling authority by contractors or other authorised persons, when men or machines are engaged in road marking.

The signs shall be removed as soon as the road marking work has been completed. This is the responsibility of the person in charge of the work party at the site.

Signs should be fitted to white painted, plain galvanised steel or plain aluminium stands. Under weather conditions likely to be experienced on all but very rare occasions the stands must be stable when:

- the centre of a TW - 1.4A diamond sign is located at least 1.25 m above the road surface, or
- the underside of a TW - 1.4B sign combination is located at least 1.0 m above the road surface.

Location: Normally, TW - 1.4 signs should be erected clear of the traffic lanes. Where this is not practicable they shall be located so as to be clearly visible to approaching drivers and not present an obstruction to traffic.

The NZTA CODE OF PRACTICE FOR TEMPORARY TRAFFIC MANAGEMENT specifies location and layout requirements for Temporary Warning signs. Special attention must be given to ensuring that the minimum clear visibility and spacing distances between the advance sign and the start of the road works site are achieved.



TW - 1.4B

For dimensions refer to

<http://www.nzta.govt.nz/resources/traffic-control-devices-manual/sign-specifications/>



TW - 1.5A *

* reflectorised fluorescent
orange background

Policy: A TW -1.5 sign combination consists of a TW - 1 ROAD WORKS sign and a supplementary sign ON SIDE ROAD.

TW - 1.5 advance warning signs may only be erected by employees of the controlling authority, or with permission of the controlling authority by contractors or other authorised persons, when a work site on a side road is located so close to an intersection that some, or all, of the advance warning signs cannot be located at the correct distances from the work site.

TW - 1.5 signs may also be erected by employees of a railway operator to facilitate maintenance of level crossing alarms.

At any time work is not in progress on the roadway, ie. during meal breaks and overnight, and the road surface is in a reasonably negotiable condition, the signs shall be covered or removed. This is the responsibility of the person in charge of the work party at the site.

The signs shall be removed as soon as the work has been completed. This is the responsibility of the person in charge of the work party at the site.

Signs should be fitted to white painted, plain galvanised steel or plain aluminium stands. Under weather conditions likely to be experienced on all but very rare occasions the stands must be stable when:

- the centre of a TW - 1.5A diamond sign is located at least 1.25 m above the road surface, or
- the underside of a TW - 1.5B sign combination is located at least 1.0 m above the road surface.

Location: Normally, TW - 1.5 signs should be erected clear of the traffic lanes. Where this is not practicable they shall be located so as to be clearly visible to approaching drivers and not present an obstruction to traffic.

The NZTA CODE OF PRACTICE FOR TEMPORARY TRAFFIC MANAGEMENT specifies location and layout requirements for Temporary Warning signs. Special attention must be given to ensuring that the minimum clear visibility and spacing distances between the advance sign and the start of the road works site are achieved.



TW - 1.5B

For dimensions refer to

<http://www.nzta.govt.nz/resources/traffic-control-devices-manual/sign-specifications/>



TW - 1.6A *

* reflectorised fluorescent
orange background

Policy: A TW - 1.6 sign combination consists of a TW - 1 ROAD WORKS sign and a supplementary sign SHOULDER CLOSED.

TW - 1.6 advance warning signs may only be erected by employees of the controlling authority or with permission of the controlling authority by contractors or other authorised persons, when a trafficable road shoulder is closed off by road works.

The signs shall be removed as soon as the work has been completed. This is the responsibility of the person in charge of the work party at the site.

Signs should be fitted to white painted, plain galvanised steel or plain aluminium stands. Under weather conditions likely to be experienced on all but very rare occasions the stands must be stable when:

- the centre of a TW - 1.6A diamond sign is located at least 1.25 m above the road surface, or
- the underside of a TW - 1.6B sign combination is located at least 1.0 m above the road surface.

Location: Normally, TW - 1.6 signs should be erected clear of the traffic lanes. Where this is not practicable they shall be located so as to be clearly visible to approaching drivers and not present an obstruction to traffic.

The NZTA CODE OF PRACTICE FOR TEMPORARY TRAFFIC MANAGEMENT specifies location and layout requirements for Temporary Warning signs. Special attention must be given to ensuring that the minimum clear visibility and spacing distances between the advance sign and the start of the road works site are achieved.



TW - 1.6B

For dimensions refer to

<http://www.nzta.govt.nz/resources/traffic-control-devices-manual/sign-specifications/>



TW - 1.7A *

* reflectorised fluorescent
orange background

Policy: A TW - 1.7 sign combination consists of a TW - 1 ROAD WORKS sign and a supplementary sign SURVEYING.

TW - 1.7 advance warning signs may only be erected by employees of the controlling authority, or with permission of the controlling authority, by other persons conducting surveys within the road reserve. The signs should be displayed only when the survey party is actually on the roadway or close to the roadway edge.

At times when work has ceased, eg. during meal breaks and overnight, and when the survey party has moved from the road reserve the signs shall be removed. This is the responsibility of the person in charge of the survey party.

Signs should be fitted to white painted, plain galvanised steel or plain aluminium stands. Under weather conditions likely to be experienced on all but very rare occasions the stands must be stable when:

- the centre of a TW - 1.7A diamond sign is located at least 1.25 m above the road surface, or
- the underside of a TW - 1.7B sign combination is located at least 1.0 m above the road surface.

Location: Normally, TW - 1.7 signs should be erected clear of the traffic lanes. Where this is not practicable they shall be located so as to be clearly visible to approaching drivers and not present an obstruction to traffic.

The NZTA CODE OF PRACTICE FOR TEMPORARY TRAFFIC MANAGEMENT specifies location and layout requirements for Temporary Warning signs. Special attention must be given to ensuring that the minimum clear visibility and spacing distances between the advance sign and the start of the road works site are achieved.



TW - 1.7B



For dimensions refer to

<http://www.nzta.govt.nz/resources/traffic-control-devices-manual/sign-specifications/>



TW - 1.8A *

* reflectorised fluorescent
orange background

Policy: A TW - 1.8 sign combination consists of a TW - 1 ROAD WORKS sign and a supplementary sign BRIDGE REPAIRS.

TW - 1.8 advance warning signs may only be erected by employees of the controlling authority, or with permission of the controlling authority by contractors or other authorised persons, when maintenance work is being undertaken on a bridge.

The signs shall be removed as soon as the work has been completed. This is the responsibility of the person in charge of the work party at the site.

Signs should be fitted to white painted, plain galvanised steel or plain aluminium stands. Under weather conditions likely to be experienced on all but very rare occasions the stands must be stable when:

- the centre of a TW - 1.8A diamond sign is located at least 1.25 m above the road surface, or
- the underside of a TW - 1.8B sign combination is located at least 1.0 m above the road surface.

Location: Normally, TW - 1.8 signs should be erected clear of the traffic lanes. Where this is not practicable they shall be located so as to be clearly visible to approaching drivers and not present an obstruction to traffic.

The NZTA CODE OF PRACTICE FOR TEMPORARY TRAFFIC MANAGEMENT specifies location and layout requirements for Temporary Warning signs. Special attention must be given to ensuring that the minimum clear visibility and spacing distances between the advance sign and the start of the road works site are achieved.



TW - 1.8B

For dimensions refer to

<http://www.nzta.govt.nz/resources/traffic-control-devices-manual/sign-specifications/>



TW - 2A

LEGEND	:	<i>black</i>
BACKGROUND	:	<i>reflectorised fluorescent orange</i>
BORDERS	:	<i>black</i>

Policy: A TW - 2 sign consists of an OTHER HAZARD symbol displayed on a standard TW sign plate.

TW - 2 advance warning signs are only erected in combination with approved supplementary signs that indicate, in words, certain specific temporary hazards. The only approved sign combinations are TW - 2.1 FLOODING, TW - 2.2 WASHOUT, TW - 2.3 LINEMEN, TW - 2.4 BLASTING, TW - 2.5 TREE FELLING, TW - 2.6 LOGGING RUCKS, TW - 2.7 TRUCKS CROSSING, TW - 2.8 NO ROAD MARKING, TW - 2.9 SIGNALS CHANGED, TW - 2.10 SIGNALS NOT WORKING, TW - 2.11 NEW ROAD LAYOUT, TW - 2.12 TRAFFIC SURVEY, TW - 2.13 VULNERABLE ROAD USER EVENT, TW - 2.14 ACCIDENT, TW - 2.15 FIRE, TW - 2.16 VULNERABLE ROAD USERS AHEAD and TW - 2.17 FUNERAL.

NOTE: TW - 2 SIGNS MUST NEVER BE ERECTED WITHOUT AN APPROVED WORDED SUPPLEMENTARY SIGN TO IDENTIFY THE SPECIFIC TEMPORARY HAZARD.



TW - 2B

LEGEND	:	<i>black</i>
BACKGROUND	:	<i>reflectorised fluorescent orange</i>
BORDERS	:	<i>black</i>

Refer to Figures 5.1 and 5.2 for details of the standard TW sign plates

For dimensions refer to _____

<http://www.nzta.govt.nz/resources/traffic-control-devices-manual/sign-specifications/>



TW - 2.1A *

* reflectorised fluorescent orange background

Policy: A TW - 2.1 sign combination consists of a TW - 2 OTHER HAZARD sign and a supplementary sign FLOODING.

TW - 2.1 advance warning the controlling authority, police, or other authorised persons and should be used wherever surface water on the roadway creates a hazard. A depth of a few centimetres can be dangerous.

As soon as the surface flooding is no longer a hazard the signs shall be removed. While this is primarily the responsibility of the organisation that erected the signs, any other authorised person should turn the sign away from approaching traffic when the warning is no longer applicable.

Signs should be fitted to white painted, plain galvanised steel or plain aluminium stands. Under weather conditions likely to be experienced on all but very rare occasions the stands must be stable when:

- the centre of a TW - 2.1A diamond sign is located at least 1.25 m above the road surface, or
- the underside of a TW - 2.1B sign combination is located at least 1.0 m above the road surface.

Location: Normally, TW - 2.1 signs should be erected clear of the traffic lanes. Where this is not practicable they shall be located so as to be clearly visible to approaching drivers and not present an obstruction to traffic.

The NZTA CODE OF PRACTICE FOR TEMPORARY TRAFFIC MANAGEMENT specifies location and layout requirements for Temporary Warning signs. Special attention must be given to ensuring that the minimum clear visibility and spacing distances between the advance sign and the start of the road works site are achieved.



TW - 2.1B

For dimensions refer to

<http://www.nzta.govt.nz/resources/traffic-control-devices-manual/sign-specifications/>



TW - 2.2A *

* reflectorised fluorescent
orange background

Policy: A TW - 2.2 sign combination consists of a TW - 2 OTHER HAZARD sign and a supplementary sign WASHOUT.

TW - 2.2 advance warning signs may only be erected by employees of the controlling authority or police and should be used wherever a portion of the road has been eroded or fallen away thus reducing the road width available to traffic. Edge marker posts should be used to indicate the edge of the usable roadway.

As soon as the full width of the roadway is available to traffic the signs shall be removed. This is the responsibility of the controlling authority.

Signs should be fitted to white painted, plain galvanised steel or plain aluminium stands. Under weather conditions likely to be experienced on all but very rare occasions the stands must be stable when:

- the centre of a TW - 2.2A diamond sign is located at least 1.25 m above the road surface, or
- the underside of a TW - 2.2B sign combination is located at least 1.0 m above the road surface.

Location: Normally, TW - 2.2 signs should be erected clear of the traffic lanes. Where this is not practicable they shall be located so as to be clearly visible to approaching drivers and not present an obstruction to traffic.

The NZTA CODE OF PRACTICE FOR TEMPORARY TRAFFIC MANAGEMENT specifies location and layout requirements for Temporary Warning signs. Special attention must be given to ensuring that the minimum clear visibility and spacing distances between the advance sign and the start of the road works site are achieved.



TW - 2.2B

For dimensions refer to

<http://www.nzta.govt.nz/resources/traffic-control-devices-manual/sign-specifications/>



TW - 2.3A *

* reflectorised fluorescent
orange background

Policy: A TW - 2.3 sign combination consists of a TW - 2 OTHER HAZARD sign and a supplementary sign LINEMEN.

TW - 2.3 advance warning signs may only be erected by employees of a telecommunications network operator or an electricity transmission agency. The signs should only be displayed when men or machines are working on overhead lines or poles adjacent to the roadway.

At any time work is not in progress on the overhead lines or poles, ie during meal breaks, the signs shall be covered or removed. This is the responsibility of the person in charge of the work party at the site.

Signs should be fitted to white painted, plain galvanised steel or plain aluminium stands. Under weather conditions likely to be experienced on all but very rare occasions the stands must be stable when:

- the centre of a TW - 2.3A diamond sign is located at least 1.25 m above the road surface, or
- the underside of a TW - 2.3B sign combination is located at least 1.0 m above the road surface.

Location: Normally, TW - 2.3 signs should be erected clear of the traffic lanes. Where this is not practicable they shall be located so as to be clearly visible to approaching drivers and not present an obstruction to traffic.

The NZTA CODE OF PRACTICE FOR TEMPORARY TRAFFIC MANAGEMENT specifies location and layout requirements for Temporary Warning signs. Special attention must be given to ensuring that the minimum clear visibility and spacing distances between the advance sign and the start of the road works site are achieved.



TW - 2.3B

For dimensions refer to

<http://www.nzta.govt.nz/resources/traffic-control-devices-manual/sign-specifications/>



TW - 2.4A *

* reflectorised fluorescent
orange background

Policy: A TW - 2.4 sign combination consists of a TW - 2 OTHER HAZARD sign and a supplementary sign BLASTING.

TW - 2.4 advance warning signs may only be erected by employees of the controlling authority or with permission of the controlling authority by contractors and other authorised persons.

The signs should be used to indicate blasting operations in hand on or near the road and where there is a danger to road users from flying debris.

Traffic controllers using TW - 33 STOP/GO paddles together with TW - 15 or TW - 15.1 MANUAL TRAFFIC CONTROL signs must be employed in conjunction with TW - 2.4 signs on all road approaches to the danger area, to prevent traffic centering the danger area for the duration of each danger period.

The signs shall only remain erected for the minimum time necessary to provide full protection to road users. Decisions to stop traffic and to re-open the road to traffic are the responsibility of the person in charge of the blasting operations.

Signs should be fitted to white painted, plain galvanised steel or plain aluminium stands. Under weather conditions likely to be experienced on all but very rare occasions the stands must be stable when:

- the centre of a TW - 2.4A diamond sign is located at least 1.25 m above the road surface, or
- the underside of a TW - 2.4B sign combination is located at least 1.0 m above the road surface.

Location: Normally, TW - 2.4 signs should be erected clear of the traffic lanes. Where this is not practicable they shall be located so as to be clearly visible to approaching drivers and not present an obstruction to traffic.

The NZTA CODE OF PRACTICE FOR TEMPORARY TRAFFIC MANAGEMENT specifies location and layout requirements for Temporary Warning signs. Special attention must be given to ensuring that the minimum clear visibility and spacing distances between the advance sign and the start of the road works site are achieved.



TW - 2.4B

For dimensions refer to

<http://www.nzta.govt.nz/resources/traffic-control-devices-manual/sign-specifications/>



TW - 2.5A *

* reflectorised fluorescent orange background

Policy: TW - 2.5 sign combination consists of a W - 2 OTHER HAZARD sign and a supplementary sign TREE FELLING.

TW -2.5 advance warning signs may only be erected by employees of the controlling authority or with permission of the controlling authority by contractors and other authorised persons.

The signs should be used to indicate tree trimming and/or felling operations are being carried out on or near the road and there is a danger to road users from falling branches or trees.

Traffic controllers using TW - 33 STOP/GO paddles together with TW - 15 or TW - 15.1 MANUAL TRAFFIC CONTROL signs must be employed in conjunction with TW - 2.5 signs on all road approaches to the danger area, to prevent traffic entering the area for the duration of each danger period.

The signs shall only remain erected for the minimum time necessary to provide full protection to road users. Decisions to stop traffic and to re-open the road to traffic are the responsibility of the person in charge of the tree trimming and/or felling operations.

Signs should be fitted to white painted, plain galvanised steel or plain aluminium stands. Under weather conditions likely to be experienced on all but very rare occasions the stands must be stable when:

- the centre of a TW - 2.5A diamond sign is located at least 1.25 m above the road surface, or
- the underside of a TW - 2.5B sign combination is located at least 1.0 m above the road surface.

Location: Normally TW - 2.5 signs should be erected clear of the traffic lanes. Where this is not practicable they shall be located so as to be clearly visible to approaching drivers and not present an obstruction to traffic.

The NZTA CODE OF PRACTICE FOR TEMPORARY TRAFFIC MANAGEMENT specifies location and lay out requirements for Temporary Warning signs. Special attention must be given to ensuring that the minimum clear visibility and spacing distances between the advance sign and the start of the road works site are achieved.



TW - 2.5B

For dimensions refer to

<http://www.nzta.govt.nz/resources/traffic-control-devices-manual/sign-specifications/>



TW - 2.6A *

* reflectorised fluorescent
orange background

Policy: A TW - 2.6 sign combination consists of a T W - 2 OTHER HAZARD sign and a supplementary sign LOGGING TRUCKS.

TW - 2.6 advance warning signs may only be erected by employees of the controlling authority or with permission of the controlling authority by logging contractors and other authorised persons in situations where logging truck movements occur to and from a road (generally via private forestry roads or farm gateway accesses) over relatively short periods (typically four to six weeks) while small forestry blocks are being logged.

The signs shall be covered or removed overnight or when logging operations have been suspended for periods greater than four hours.

Signs should be fitted to white painted, plain galvanised steel or plain aluminium stands. Under weather conditions likely to be experienced on all but very rare occasions the stands must be stable when:

- the centre of a TW - 2.6A diamond sign is located at least 1.25 m above the road surface, or
- the underside of a TW - 2.6B sign combination is located at least 1.0 m above the road surface.

Location: Normally, TW - 2.6 signs should be erected clear of the traffic lanes. Where this is not practicable they shall be located so as to be clearly visible to approaching drivers and not present an obstruction to traffic.

The NZTA CODE OF PRACTICE FOR TEMPORARY TRAFFIC MANAGEMENT specifies location and layout requirements for Temporary Warning signs. Special attention must be given to ensuring that the minimum clear visibility and spacing distances between the advance sign and the start of the road works site are achieved.



TW - 2.6B

For dimensions refer to

<http://www.nzta.govt.nz/resources/traffic-control-devices-manual/sign-specifications/>



TW - 2.7A *

* reflectorised fluorescent orange background

Policy: A TW - 2.7 sign combination consists of a TW-7 OTHER HAZARD sign and a supplementary sign TRUCKS CROSSING.

TW - 2.7 advance warning signs may only be erected by employees of the controlling authority or with permission of the controlling authority by contractors and other authorised persons, in situations where a large number of truck movements occur and from a site adjacent to the road over a relatively short period (typically four to six weeks), while site construction or development work is in progress. The signs should not be used in urban areas or at road works sites.

The signs shall be covered or removed overnight.

Signs should be fitted to white painted, plain galvanised steel or plain aluminium stands. Under weather conditions likely to be experienced on all but very rare occasions the stands must be stable when:

- the centre of a TW - 2.7A diamond sign is located at least 1.25 m above the road surface, or
- the underside of a TW - 2.7B sign combination is located at least 1.0 m above the road surface.

Location: Normally, TW - 2.7 signs should be erected clear of the traffic lanes. Where this is not practicable they shall be located so as to be clearly visible to approaching drivers and not present an obstruction to traffic.

The NZTA CODE OF PRACTICE FOR TEMPORARY TRAFFIC MANAGEMENT specifies location and layout requirements for Temporary Warning signs. Special attention must be given to ensuring that the minimum clear visibility and spacing distances between the advance sign and the start of the road works site are achieved.



TW - 2.7B

For dimensions refer to

<http://www.nzta.govt.nz/resources/traffic-control-devices-manual/sign-specifications/>



TW - 2.8A *

* reflectorised fluorescent orange background

Policy: A TW - 2.8 sign combination consists of a TW - 2 OTHER HAZARD sign and a supplementary sign NO ROAD MARKING.

TW - 2.8 advance warning signs may only be erected by employees of the controlling authority or, with permission of the controlling authority, by contractors or other authorised persons in situations where road markings have been obliterated due to road work operations such as pavement burning and where use of the TW - 5.1 NEW SEAL sign is inappropriate because it displays flying stones and a broken windscreen. The TW - 5.1 sign should continue to be used to warn of re-surfacing operations.

The signs shall be removed when the road markings have been reinstated

Signs should be fitted to white painted, plain galvanised steel or plain aluminium stands. Under weather conditions likely to be experienced on all but very rare occasions the stands must be stable when:

- the centre of a TW - 2.8A diamond sign is located at least 1.25 m above the road surface, or
- the underside of a TW - 2.8B sign combination is located at least 1.0 m above the road surface.

Location: Normally, TW - 2.8 signs should be erected clear of the traffic lanes. Where this is not practicable they shall be located so as to be clearly visible to approaching drivers and not present an obstruction to traffic.

The NZTA CODE OF PRACTICE FOR TEMPORARY TRAFFIC MANAGEMENT specifies location and layout requirements for Temporary Warning signs. Special attention must be given to ensuring that the minimum clear visibility and spacing distances between the advance sign and the start of the road works site are achieved.



TW - 2.8B



TW - 2.9A *

* reflectorised fluorescent orange background

For dimensions refer to

<http://www.nzta.govt.nz/resources/traffic-control-devices-manual/sign-specifications/>

Policy: A TW - 2.9 sign combination consists of a TW - 2 OTHER HAZARD sign and a supplementary sign SIGNALS CHANGED.

TW - 2.9 advance warning signs may only be erected by employees of the controlling authority or with permission of the controlling authority by contractors and other authorised persons, at intersections where the traffic signal control sequence changed. The signs should remain in place for at least two weeks following the changes.

Signs should be fitted to white painted, plain galvanised steel or plain aluminium stands. Under weather conditions likely to be experienced on all but very rare occasions the stands must be stable when:

- the centre of a TW - 2.9A diamond sign is located at least 1.25 m above the road surface, or
- the underside of a TW - 2.9B sign combination is located at least 1.0 m above the road surface.

Location: Normally, TW - 2.9 signs should be erected clear of the traffic lanes. Where this is not practicable they shall be located so as to be clearly visible to approaching drivers and not present an obstruction to traffic.

The NZTA CODE OF PRACTICE FOR TEMPORARY TRAFFIC MANAGEMENT specifies location and layout requirements for Temporary Warning signs. Special attention must be given to ensuring that the minimum clear visibility and spacing distances between the advance sign and the start of the road works site are achieved.



TW - 2.9B

For dimensions refer to

<http://www.nzta.govt.nz/resources/traffic-control-devices-manual/sign-specifications/>



TW - 2.10A *

* reflectorised fluorescent orange background

Policy: A TW - 2.10 sign combination consists of a TW-2 OTHER HAZARD sign and a supplementary sign SIGNALS NOT WORKING.

TW - 2.10 advance warning signs may only be erected by employees of the controlling authority or with permission of the controlling authority by contractors or other authorised persons when a traffic signal is not operational because of a fault or maintenance work.

The signs shall be removed as soon as the traffic signal is operating. This is the responsibility of the person in charge of the work party at the site.

Signs should be fitted to white painted, plain galvanised steel or plain aluminium stands. Under weather conditions likely to be experienced on all but very rare occasions the stands must be stable when:

- the centre of a TW - 2.10A diamond sign is located at least 1.25 m above the road surface, or
- the underside of a TW - 2.10B sign combination is located at least 1.0 m above the road surface.

Location: Normally, TW - 2.10 signs should be erected clear of the traffic lanes. Where this is not practicable they shall be located so as to be clearly visible to approaching drivers and not present an obstruction to traffic.

The NZTA CODE OF PRACTICE FOR TEMPORARY TRAFFIC MANAGEMENT specifies location and layout requirements for Temporary Warning signs. Special attention must be given to ensuring that the minimum clear visibility and spacing distances between the advance sign and the start of the road works site are achieved.



TW - 2.10B

For dimensions refer to

<http://www.nzta.govt.nz/resources/traffic-control-devices-manual/sign-specifications/>



TW - 2.11A *

* reflectorised fluorescent orange background

Policy: A TW - 2.11 sign combination consists of a TW - 2 OTHER HAZARD sign and a supplementary sign NEW ROAD LAYOUT.

TW - 2.11 advance warning signs may only be erected by employees of the controlling authority, or with permission of the controlling authority by contractors or other authorised persons, when a section of road alignment or an existing intersection that has been altered by road works is first opened to traffic. The signs should remain in place for at least two weeks after the opening of the new alignment.

Signs should be fitted to white painted, plain galvanised steel or plain aluminium stands. Under weather conditions likely to be experienced on all but very rare occasions the stands must be stable when:

- the centre of a TW - 2.11A diamond sign is located at least 1.25 m above the road surface, or
- the underside of a TW - 2.11B sign combination is located at least 1.0 m above the road surface.

Location: Normally, TW - 2.11 signs should be erected clear of the traffic lanes. Where this is not practicable they shall be located so as to be clearly visible to approaching drivers and not present an obstruction to traffic.

The NZTA CODE OF PRACTICE FOR TEMPORARY TRAFFIC MANAGEMENT specifies location and layout requirements for Temporary Warning signs. Special attention must be given to ensuring that the minimum clear visibility and spacing distances between the advance sign and the start of the road works site are achieved.



TW - 2.11B

For dimensions refer to

<http://www.nzta.govt.nz/resources/traffic-control-devices-manual/sign-specifications/>



TW - 2.12A *

* reflectorised fluorescent orange background

Policy: A TW - 2.12 sign combination consists of a TW - 2 OTHER HAZARD sign and a supplementary sign TRAFFIC SURVEY.

TW - 2.12 advance warning signs may only be erected by employees of the controlling authority or with permission of the controlling authority by contractors or other authorised persons, on the approaches to roadside sites for the duration of traffic survey driver interviews.

Each TW - 2.12 sign is to be augmented with a TW - 17 PLEASE STOP ON REQUEST traffic direction sign. A TW - 16 THANK YOU sign is to be erected downstream of the interview site.

Signs should be fitted to white painted, plain galvanised steel or plain aluminium stands. Under weather conditions likely to be experienced on all but very rare occasions the stands must be stable when:

- the centre of a TW - 2.12A diamond sign is located at least 1.25 m above the road surface, or
- the underside of a TW - 2.12B sign combination is located at least 1.0 m above the road surface.

Location: Normally, TW - 2.1 signs should be erected clear of the traffic lanes. Where this is not practicable they shall be located so as to be clearly visible to approaching drivers and not present an obstruction to traffic.

The NZTA CODE OF PRACTICE FOR TEMPORARY TRAFFIC MANAGEMENT specifies location and layout requirements for Temporary Warning signs. Special attention must be given to ensuring that the minimum clear visibility and spacing distances between the advance sign and the start of the road works site are achieved.



TW - 2.12B

Policy: A TW - 2.13 sign combination consists of a TW - 2 OTHER HAZARD sign and a supplementary sign indicating the type of vulnerable road user event, ie. CYCLE RACE, RUNNERS or WALKERS.

TW - 2.13 advance warning signs may only be erected by employees of the controlling authority or, with written permission of the controlling authority, by event organisers and other authorised person. These signs shall be removed as soon as the event is completed, this is the responsibility of the person(s) authorised to erect the signs.

Signs should be fitted to white painted, plain galvanised steel or plain aluminium stands. Under weather conditions likely to be experienced on all but very rare occasions the stands must be stable when:

- the centre of a TW - 2.13A diamond sign is located at least 1.25 m above the road surface, or
- the underside of a TW - 2.13B sign combination is located at least 1.0 m above the road surface.

Location: Normally TW - 2.13 signs should be erected clear of the traffic lanes. Where this is not practicable they shall be located so as to be clearly visible to approaching drivers and not present an obstruction to traffic.

The Transit New Zealand publication CODE OF PRACTICE FOR TEMPORARY TRAFFIC MANAGEMENT specifies location and layout requirements for Temporary Warning signs. Special attention must be given to ensuring that the minimum clear visibility and spacing distances between the advance sign and the vulnerable road user event site is achieved.



CYCLE RACE

TW - 2.13.1A *

** reflectorised fluorescent orange background*



RUNNERS

TW - 2.13.2A *

** reflectorised fluorescent orange background*

For dimensions refer to <http://www.nzta.govt.nz/resources/traffic-control-devices-manual/sign-specifications/>



CYCLE RACE

TW - 2.13.1B



WALKERS

TW - 2.13.3B

For dimensions refer to

<http://www.nzta.govt.nz/resources/traffic-control-devices-manual/sign-specifications/>



TW - 2.14A *

* reflectorised fluorescent orange background

Policy: A TW - 2.14 sign combination consists of a TW - 2 OTHER HAZARD sign and a supplementary sign ACCIDENT.

TW - 2.14 advance warning signs may only be erected by the police or persons authorised by them at major traffic accident sites.

As soon as the police authorise normal traffic operation can be resumed the signs shall be removed. This is the responsibility of the person(s) or organisation that erected the signs.

Signs should be fitted to white painted, plain galvanised steel or plain aluminium stands. Under weather conditions likely to be experienced on all but very rare occasions the stands must be stable when:

- the centre of a TW - 2.14A diamond sign is located at least 1.25 m above the road surface, or
- the underside of a TW - 2.14B sign combination is located at least 1.0 m above the road surface.

Location: Normally, TW - 2.14 signs should be erected clear of the traffic lanes. Where this is not practicable they shall be located so as to be clearly visible to approaching drivers and not present an obstruction to traffic.

The NZTA CODE OF PRACTICE FOR TEMPORARY TRAFFIC MANAGEMENT specifies location and layout requirements for Temporary Warning signs. Special attention must be given to ensuring that the minimum clear visibility and spacing distances between the advance sign and the start of the road works site are achieved.



TW - 2.14B

For dimensions refer to _____

<http://www.nzta.govt.nz/resources/traffic-control-devices-manual/sign-specifications/>



TW - 2.15A *

** reflectorised fluorescent
orange background*

Policy: A TW - 2.15 sign combination consists of a TW - 2 OTHER HAZARD sign and a supplementary sign FIRE.

TW - 2.15 advance warning signs may only be erected by employees of the controlling authority fire services, police or other authorised persons and should be used wherever fire fighting operations, or drifting smoke, presents a hazard to normal traffic operations.

The signs shall be removed as soon as normal traffic operation can be resumed. This is the responsibility of the person(s) or organisation that erected the signs.

Signs should be fitted to white painted, plain galvanised steel or plain aluminium stands. Under weather conditions likely to be experienced on all but very rare occasions the stands must be stable when:

- the centre of a TW - 2.15A diamond sign is located at least 1.25 m above the road surface, or
- the underside of a TW - 2.15B sign combination is located at least 1.0 m above the road surface.

Location: Normally, TW - 2.15 signs should be erected clear of the traffic lanes. Where this is not practicable they shall be located so as to be clearly visible to approaching drivers and not present an obstruction to traffic.

The NZTA CODE OF PRACTICE FOR TEMPORARY TRAFFIC MANAGEMENT specifies location and layout requirements for Temporary Warning signs. Special attention must be given to ensuring that the minimum clear visibility and spacing distances between the advance sign and the start of the road works site are achieved.



TW - 2.15B



TW - 2.16.1 *



TW - 2.16.2 *

* reflectorised fluorescent orange background



TW - 2.16.3 *

For dimensions refer to <http://www.nzta.govt.nz/resources/traffic-control-devices-manual/sign-specifications/>

Policy: A TW - 2.16 sign combination consists of a TW - 2 OTHER HAZARD sign and a supplementary sign indicating vulnerable road users taking part in a long distance event, ie. CYCLISTS AHEAD, RUNNERS AHEAD or WALKERS AHEAD.

TW - 2.16 advance warning signs may, with the written permission of the controlling authority, be erected on the front and rear of pilot vehicles accompanying the vulnerable road users by event organisers and other authorised persons.

Location: The NZTA CODE OF PRACTICE FOR TEMPORARY TRAFFIC MANAGEMENT specifies location and layout requirements for Temporary Warning signs. Special attention must be given to ensuring that the minimum clear visibility and spacing distances between the advance sign and the vulnerable road users are achieved.

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TW - 2.17A *

* reflectorised fluorescent orange background

For dimensions refer to _____

<http://www.nzta.govt.nz/resources/traffic-control-devices-manual/sign-specifications/>

Policy: A TW - 2.17 sign combination consists of a TW - 2 OTHER HAZARD sign and a supplementary sign FUNERAL.

TW - 2.17 advance warning signs may only be erected by employees of the controlling authority or, with written permission of the controlling authority by the organisers of funerals and other authorised persons, when it is likely that the funeral activities will present a hazard to normal traffic operations.

The signs shall be removed as soon as the funeral activities no longer present a hazard to normal traffic operations, this is the responsibility of the person(s) authorised to erect the signs.

Signs should be fitted to white painted, plain galvanised steel or plain aluminium stands. Under weather conditions likely to be experienced on all but very rare occasions the stands must be stable when:

- the centre of a TW - 2.17A diamond sign is located at least 1.25 m above the road surface, or
- the underside of a TW - 2.17B sign combination is located at least 1.0 m above the road surface.

Location: Normally TW - 2.17 signs should be erected clear of the traffic lanes. Where this is not practicable they shall be located so as to be clearly visible to approaching drivers and not present an obstruction to traffic.

The NZTA CODE OF PRACTICE FOR TEMPORARY TRAFFIC MANAGEMENT specifies location and layout requirements for Temporary Warning signs. Special attention must be given to ensuring that the minimum clear visibility and spacing distances between the advance sign and the funeral activities are achieved.



TW - 2.17B



TW - 2.18A *

* reflectorised fluorescent orange background

For dimensions refer to

<http://www.nzta.govt.nz/resources/traffic-control-devices-manual/sign-specifications/>

Policy: A TW - 2.18 sign combination consists of a TW - 2 OTHER HAZARD sign and a supplementary sign FILM CREW.

TW - 2.18 advance warning signs may only be erected by employees of the controlling authority or, with written permission of the controlling authority by film makers and other authorised persons, when it is likely that film making activities will present a hazard to normal traffic operations.

The signs shall be removed as soon as the film making activities no longer present a hazard to normal traffic operations, this is the responsibility of the person(s) authorised to erect the signs.

Signs should be fitted to white painted, plain galvanised steel or plain aluminium stands. Under weather conditions likely to be experienced on all but very rare occasions the stands must be stable when:

- the centre of a TW - 2.18A diamond sign is located at least 1.25 m above the road surface, or
- the underside of a TW - 2.18B sign combination is located at least 1.0m above the road surface.

Location: Normally TW - 2.18 signs should be erected clear of the traffic lanes. Where this is not practicable they shall be located so as to be clearly visible to approaching drivers and not present an obstruction to traffic.

The NZTA CODE OF PRACTICE FOR TEMPORARY TRAFFIC MANAGEMENT specifies location and layout requirements for Temporary Warning signs. Special attention must be given to ensuring that the minimum clear visibility and spacing distances between the advance sign and the start of the road works site are achieved.



TW - 2.18B

Policy: A TW - 3 sign consists of a SLIPS symbol displayed on a standard TW sign plate.

TW - 3 advance warning signs may only be erected by employees of the controlling authority, police, or other authorised persons and should be used wherever part of the roadway is affected by slips or other fallen debris.

Where several such sections of road occur in close proximity the first TW - 3 sign may be augmented with an additional supplementary plate NEXT "___" km. Refer to the TW - 1.1 sign specification for details of the NEXT "___" km sign.

As soon as the roadway is cleared of the slip debris the signs must be removed. This is the responsibility of the person in charge of the work party at the site.

Signs should be fitted to white painted, plain galvanised steel or plain aluminium stands. Under weather conditions likely to be experienced on all but very rare occasions the stands must be stable when:

- the centre of a TW - 3A diamond sign is located at least 1.25 m above the road surface, or
- the underside of a TW - 3B sign is located at least 1.0 m above the road surface.

Location: Normally, TW - 3 signs should be erected clear of the traffic lanes. Where this is not practicable they shall be located so as to be clearly visible to approaching drivers and not present an obstruction to traffic.

The NZTA CODE OF PRACTICE FOR TEMPORARY TRAFFIC MANAGEMENT specifies location and layout requirements for Temporary Warning signs. Special attention must be given to ensuring that the minimum clear visibility and spacing distances between the advance sign and the start of the road works site are achieved.



TW - 3A*

* reflectorised fluorescent orange background



TW - 3B

Refer to Figures 5.1 and 5.2 for details of the standard TW sign plates.

For dimensions refer to <http://www.nzta.govt.nz/resources/traffic-control-devices-manual/sign-specifications/>

Policy: A TW - 4 sign consists of a SKIDDING CAR symbol displayed on a standard TW sign plate.

TW - 4 advance warning signs may only be erected by employees of the controlling authority, or by others with its permission.

These signs may be used in the following circumstances:

- (a) Where road works machines carry loose materials onto the roadway surface temporarily increasing the risk of it becoming slippery when wet.
- (b) Where a road surface under construction or repair suddenly changes from seal to loose material temporarily increasing the risk of skidding.
- (c) Where the skid resistance of the road surface is found to be below the threshold level as defined in Transit NZ specification T/10 **and** as a result the risk of drivers losing control there is abnormally high.

The signs must be removed as soon as the surface is cleaned or repaired. In the case outlined in (c) above, this would normally be within 9 months of the discovery of the risk.

Where other surface defects not of a temporary nature, or environmental issues (eg: shading), cause the roadway surface to become slippery when wet, PW - 41.2 signs must be used instead.

Signs should be fitted to white painted posts, or plain galvanised steel or aluminium stands. Under weather conditions likely to be experienced on all but very rare occasions the stands must be stable when:

- the centre of a TW - 4A diamond sign is located at least 1.25 m above the road surface, or
- the underside of a TW - 4B sign is located at least 1.0 m above the road surface.

Location: Normally, TW - 4 signs should be erected clear of the traffic lanes. Where this is not practicable they shall be located so as to be clearly visible to approaching drivers and not present an obstruction to traffic.

The NZTA CODE OF PRACTICE FOR TEMPORARY TRAFFIC MANAGEMENT specifies location and layout requirements for Temporary Warning signs. Special attention must be given to ensuring that the minimum clear visibility and spacing distances between the advance sign and the start of the road works site are achieved.

Additional TW - 4 signs, spaced no more than 1 km apart, should be erected along the affected section of road as appropriate.

Where several such sections of road occur in close proximity, the first TW - 4 sign may be augmented with an additional supplementary plate NEXT " " km. Refer to the TW - 1.1 sign specification for details of this plate.



TW - 4A*

* reflectorised fluorescent orange background



TW - 4B

Refer to Figures 5.1 and 5.2 for details of the standard TW sign plates.

For dimensions refer to <http://www.nzta.govt.nz/resources/traffic-control-devices-manual/sign-specifications/>

For dimensions refer to

<http://www.nzta.govt.nz/resources/traffic-control-devices-manual/sign-specifications/>



TW - 4.1A*

* reflectorised fluorescent orange background

Policy: A TW - 4.1 sign combination consists of a TW - 4 SLIPPERY SURFACE sign and a supplementary sign ICE / GRIT.

TW 4.1 signs may be erected only by employees of the controlling authority, or with permission of the controlling authority by contractors or other authorised persons, when grit is spread onto the roadway surface when ice forms.

Additional TW - 4.1 signs, spaced no more than 2 km apart, should be erected along sections of road when grit has been spread on the roadway surface when ice forms.

Where several such sections of road occur in close proximity the first TW - 4.1 sign may be augmented with an additional supplementary plate NEXT "___" km. Refer to the TW - 1.1 sign specification for details of the NEXT "___" km sign.

Where a TW - 4.1 sign is to be erected near a PW - 41.1 SLIPPERY SURFACE - WHEN FROSTY sign it should be located approximately 20 to 50 m past the PW - 41.1 sign by and in such a position that approaching drivers are able to view both signs at the same time.

The signs must be removed as soon as the roadway surface is cleared of grit. This is the responsibility of the person in charge of the work party at the site.

Signs should be fitted to white painted, plain galvanised steel or plain aluminium stands. Under weather conditions likely to be experienced on all but very rare occasions the stands must be stable when:

- the centre of a TW - 4.1A diamond sign is located at least 1.25 m above the road surface, or
- the underside of a TW - 4.1B sign combination is located at least 1.0 m above the road surface.

Location: Normally, TW - 4.1 signs should be erected clear of the traffic lanes. Where this is not practicable they shall be located so as to be clearly visible to approaching drivers and not present an obstruction to traffic.

The NZTA CODE OF PRACTICE FOR TEMPORARY TRAFFIC MANAGEMENT specifies location and layout requirements for Temporary Warning signs. Special attention must be given to ensuring that the minimum clear visibility and spacing distances between the advance sign and the start of the road works site are achieved.



TW - 4.1B

Refer to Figures 5.1, 5.2, 5.3 and 5.4 for details of standard TW sign plates.

Policy: A T W - 5 sign consists of a FLYING STONES/BROKEN WINDSCREEN symbol displayed on a standard TW sign plate.

TW - 5 advance warning signs may only be erected by employees of the controlling authority or, with permission of the controlling authority, by contractors or other authorised persons. The signs should be used when a section of normally sealed road temporarily has a gravel surface. Because this is a more specific warning than the TW - 1 ROAD WORKS sign it should be used in preference to that sign whenever the main hazard is the gravel surface.

Additional TW - 5 signs, spaced no more than 1 km apart, should be erected along a section of normally sealed road.

Where several such sections of road occur in close proximity the first TW - 5 sign may be augmented with an additional supplementary plate NEXT "___" km. Refer to the TW - 1.1 sign specification for details of the NEXT "___" km sign.

The signs should be replaced with TW - 5.1 NEW SEAL signs as soon as the surface has been sealed. This is the responsibility of the person in charge of the work party at the site.

Signs should be fitted to white painted, plain galvanised steel or plain aluminium stands. Under weather conditions likely to be experienced on all but very rare occasions the stands must be stable when:

- the centre of a TW - 5A diamond sign is located at least 1.25 m above the road surface, or
- the underside of a TW - 5B sign is located at least 1.0 m above the road surface.

Location: Normally, TW - 5 signs should be erected clear of the traffic lanes. Where this is not practicable they shall be located so as to be clearly visible to approaching drivers and not present an obstruction to traffic.

The NZTA CODE OF PRACTICE FOR TEMPORARY TRAFFIC MANAGEMENT specifies location and layout requirements for Temporary Warning signs. Special attention must be given to ensuring that the minimum clear visibility and spacing distances between the advance sign and the start of the road works site are achieved.



TW - 5A*

* reflectorised fluorescent orange background



TW - 5B

Refer to Figures 5.1 and 5.2 for details of the standard TW sign plates.

For dimensions refer to <http://www.nzta.govt.nz/resources/traffic-control-devices-manual/sign-specifications/>



TW - 5.1A*

* reflectorised fluorescent orange background

For dimensions refer to

<http://www.nzta.govt.nz/resources/traffic-control-devices-manual/sign-specifications/>

Policy: A TW - 5.1 sign combination consists of a TW - 5 GRAVEL SURFACE sign and a supplementary sign NEW SEAL.

TW - 5.1 advance warning signs may only be erected by employees of the controlling authority or, with permission of the controlling authority, by contractors or other authorised persons. The signs should be used as soon as newsealing work has been completed and remain in position until all loose chip has been removed and new pavement markings have been installed.

Additional TW - 5.1 signs, spaced no more than 1 km apart, should be erected along a section of newsealing work.

Where several such sections of road occur in close proximity the first TW - 5.1 sign may be augmented with an additional supplementary plate NEXT "___" km. Refer to the TW - 1.1 sign specification for details of the NEXT "___" km sign.

Signs should be fitted to white painted, plain galvanised steel or plain aluminium stands. Under weather conditions likely to be experienced on all but very rare occasions the stands must be stable when:

- the centre of a TW - 5.1A diamond sign is located at least 1.25 m above the road surface, or
- the underside of a TW - 5.1B sign combination is located at least 1.0 m above the road surface.

Location: Normally, TW - 5.1 signs should be erected clear of the traffic lanes. Where this is not practicable they shall be located so as to be clearly visible to approaching drivers and not present an obstruction to traffic.

The NZTA CODE OF PRACTICE FOR TEMPORARY TRAFFIC MANAGEMENT specifies location and layout requirements for Temporary Warning signs. Special attention must be given to ensuring that the minimum clear visibility and spacing distances between the advance sign and the start of the road works site are achieved.



TW - 5.1B

Refer to Figures 5.1, 5.2, 5.3 and 5.4 for details of the standard TW sign plates.



For dimensions refer to

<http://www.nzta.govt.nz/resources/traffic-control-devices-manual/sign-specifications/>



TW - 5.2A*

* reflectorised fluorescent
orange background

Policy: A TW - 5.2 sign combination consists of a TW - 5 GRAVEL SURFACE sign and a supplementary sign SEAL REPAIRS.

TW - 5.2 advance warning signs may only be erected by employees of the controlling authority or with permission of the controlling authority, by contractors or other authorised persons. The signs should be used when multiple seal repair patching work is being carried out on a section of road less than 1 km in length.

Where the length of road under repair is greater than one kilometre TW - 5.2 signs must be repeated 1 km.

Where several such sections of road occur in close proximity the first TW - 5.2 sign may be augmented with an additional supplementary plate NEXT "___" km. Refer to the W - 1.1 sign specification for details of the NEXT "___" km sign, and may be augmented with another supplementary plate NEXT 1 km.

TW - 5.2 signs should remain in position until all loose chip has been removed.

Signs should be fitted to white painted, plain galvanised steel or plain aluminium stands. Under weather conditions likely to be experienced on all but very rare occasions the stands must be stable when:

- the centre of a TW - 5.2A diamond sign is located at least 1.25 m above the road surface, or
- the underside of a TW - 5.2B sign combination is located at least 1.0 m above the road surface.

Location: Normally, signs should be erected clear of the traffic lanes. Where this is not practicable they shall be located so as to be clearly visible to approaching drivers and not present an obstruction to traffic.

The NZTA CODE OF PRACTICE FOR TEMPORARY TRAFFIC MANAGEMENT specifies location and layout requirements for Temporary Warning signs. Special attention must be given to ensuring that the minimum clear visibility and spacing distances between the advance sign and the start of the road works site are achieved.



TW - 5.2B

Refer to Figures 5.1, 5.2, 5.3 and 5.4 for details of the standard TW sign plates.

Policy: A TW - 6 sign consists of a COW symbol displayed on a standard TW sign plate. The alternative TW - 6.1 sign displays a SHEEP symbol instead.

TW - 6 (TW - 6.1) advance warning signs may only be erected by employees of the controlling authority or with written permission of the controlling authority, by farmers and other authorised persons. The sign should be used where driven stock crosses, or travels short distances along a roadway at infrequent intervals (say greater than 2 days) and in such a location as to cause a traffic hazard. The signs should only be displayed when stock is actually within the road reserve.

When the frequency of such stock movements is greater (say on a regular daily basis - often perhaps several times a day) or, where the lack of fences, walls, etc along the road reserve results in the continuous presence of stock on the road permanent PW - 37 (PW - 37.1) STOCK signs should be used.

The particular symbol displayed, ie. COW or SHEEP, should be chosen to suit the predominant stock type that will be driven on the road.

The signs shall be removed as soon as the stock has been moved from the road reserve. This is the responsibility of the person authorised to erect the sign.

Signs should be fitted to white painted, plain galvanised steel or plain aluminium stands. Under weather conditions likely to be experienced on all but very rare occasions the stands must be stable when:

- the centre of a TW - 6A (TW - 6.1A) diamond sign is located at least 1.25 m above the road surface, or
- the underside of a TW - 6B (TW - 6.1B) sign is located at least 1.0 m above the road surface.

Location: Normally TW - 6 (TW - 6.1) signs should be erected clear of the traffic lanes. Where this is not practicable they shall be located so as to be clearly visible to approaching drivers and not present an obstruction to traffic.

When approaching the hazard area drivers must have an uninterrupted view of the signs over a distance called the SIGN VISIBILITY DISTANCE. The greater the operating speed, the greater this distance. Special attention must be paid to the position of signs near corners or hilltops to ensure that the required visibility distance is provided.

The WARNING DISTANCE is the minimum required distance between the sign and the start of the hazard area.

Operating Speed (km/h)	50	80	100
Sign Visibility Distance (m)	60	80	120
Warning Distance (m)	60	90	150



TW - 6A*

* reflectorised fluorescent orange background



TW - 6.1B

Refer to Figures 5.1 and 5.2 for details of the standard TW sign plates.

For dimensions refer to <http://www.nzta.govt.nz/resources/traffic-control-devices-manual/sign-specifications/>

TW - 6
TW - 6.1

STOCK - TEMPORARY (CATTLE)
STOCK - TEMPORARY (SHEEP)

Policy: A TW - 7 LANE CLOSED - TWO LANE ONE WAY ROAD sign consists of an angled ARROW symbol and a straight ARROW symbol displayed side by side on a standard square TW sign panel. A supplementary sign displaying the distance to the lane closure is added to the TW - 7B sign to form a sign combination. The symbols indicate one of the two (2) lanes is closed and traffic in that lane must merge into the adjacent lane. Left or right hand versions of the sign should be used as required.

TW - 7 sign components must never be erected separately.

TW - 7 signs may only be erected by employees of the controlling authority, or with permission of the controlling authority by contractors or other authorised persons, on four lane undivided two way roads, on four lane median divided two way roads (two lanes in each direction) and on two lane one way roads.

The signs shall be removed along with all other temporary signing as soon as the closed lane has been reopened to traffic. This is the responsibility of the person in charge of the work party at the site.

Signs should be fitted to white painted, plain galvanised steel or plain aluminium stands. Under weather conditions likely to be experienced on all but very rare occasions the stands must be stable when:

- the centre of a TW - 7A square sign is located at least 1.25 m above the road surface, or
- the underside of a TW - 7B sign combination is located at least 1.0 m above the road surface.

Location: Normally, TW - 7 signs should be erected on both sides of the road and clear of the traffic lanes. Where this is not practicable they shall be located so as to be clearly visible to approaching drivers and not present an obstruction to traffic.

The NZTA CODE OF PRACTICE FOR TEMPORARY TRAFFIC MANAGEMENT specifies location and layout requirements for Temporary Warning signs. Special attention must be given to ensuring that the minimum clear visibility and spacing distances between the advance sign and the start of the road works site are achieved.



TW - 7.A



TW - 7B

Refer to Figure 5.4 for details of the standard TW / C sign plate.

For dimensions refer to <http://www.nzta.govt.nz/resources/traffic-control-devices-manual/sign-specifications/>

Policy: A TW - 7.1 LANE CLOSED - THREE LANE ONE WAY ROAD sign consists of an angled ARROW symbol and two straight ARROW symbols displayed side by side on a standard TW square sign panel. A supplementary sign displaying the distance to the lane closure is added to the TW - 7.1B sign to form a sign combination.. The symbols indicate one of the three (3) lanes is closed and traffic in that lane must merge into the adjacent lane. Left or right hand versions of the sign should be used as required.

TW - 7.1 sign components must never be erected separately.

TW - 7.1 signs may only be erected by employees of the controlling authority, or with permission of the controlling authority by contractors or other authorised persons, on six lane median divided two way roads (three lanes in each direction) and on three lane one way roads.

The signs shall be removed along with all other temporary signing as soon as the closed lane (s) have been reopened to traffic. This is the responsibility of the person in charge of the work party at the site.

Signs should be fitted to white painted, plain galvanised steel or plain aluminium stands. Under weather conditions likely to be experienced on all but very rare occasions the stands must be stable when:

- the centre of a TW - 7.1A square sign is located at least 1.25 m above the road surface, or
- the underside of a TW - 7.1B sign combination is located at least 1.0 m above the road surface.

Location: Normally, TW - 7.1 should be erected on both sides of the road and clear of the traffic lanes. Where this is not practicable they shall be located so as to be clearly visible to approaching drivers and not present an obstruction to traffic.

The NZTA CODE OF PRACTICE FOR TEMPORARY TRAFFIC MANAGEMENT specifies location and layout requirements for Temporary Warning signs. Special attention must be given to ensuring that the minimum clear visibility and spacing distances between the advance sign and the start of the road works site are achieved.



TW - 7.1A



TW - 7.1B

Refer to Figures 5.2 and 5.4 for details of standard TW sign plates.

For dimensions refer to <http://www.nzta.govt.nz/resources/traffic-control-devices-manual/sign-specifications/>

Policy: A TW - 7.1.1 CENTRE LANE CLOSED - THREE LANE ONE WAY ROAD sign consists of two (2) straight ARROW symbols and an angled ARROW symbol displayed side by side on a standard TW square sign panel. A supplementary sign displaying the distance to the lane closure is added to the TW - 7.1.1B sign to form a sign combination. The symbols indicate that the centre lane is closed and traffic in that lane must merge into the adjacent lane. Left or right hand versions of the sign should be used as required.

TW - 7.1.1 sign components must never be erected separately.

TW - 7.1.1 signs may only be erected by employees of the controlling authority, or with permission of the controlling authority by contractors or other authorised persons, on six lane median divided two way roads (three lanes in each direction) and on three lane one way roads in areas **where the speed limit is 50 km/h or less.**

The signs shall be removed along with all other temporary signing as soon as the closed lane(s) have been reopened to traffic. This is the responsibility of the person in charge of the work party at the site.

Signs should be fitted to white painted, plain galvanised steel or plain aluminium stands. Under weather conditions likely to be experienced on all but very rare occasions the stands must be stable when:

- the centre of a TW - 7.1.1A square sign is located at least 1.25 m above the road surface, or
- the underside of a TW - 7.1.1B sign combination is located at least 1.0 m above the road surface.

Location: Normally, TW - 7.1.1 should be erected on both sides of the road and clear of the traffic lanes. Where this is not practicable they shall be located so as to be clearly visible to approaching drivers and not present an obstruction to traffic.

The NZTA CODE OF PRACTICE FOR TEMPORARY TRAFFIC MANAGEMENT specifies location and layout requirements for Temporary Warning signs. Special attention must be given to ensuring that the minimum clear visibility and spacing distances between the advance sign and the start of the road works site are achieved.



TW - 7.1.1A



TW - 7.1.1B

Refer to Figures 5.2 and 5.4 for details of standard TW sign plates.

For dimensions refer to <http://www.nzta.govt.nz/resources/traffic-control-devices-manual/sign-specifications/>

Policy: A TW - 72 LANE CLOSED - FOUR LANE ONE WAY ROAD sign consists of an angled ARROW symbol and three straight ARROW symbols displayed side by side on a special rectangular TW sign panel, and a supplementary sign displaying the distance to the lane closure. The symbols indicate one of the four (4) lanes is closed and traffic in that lane must move to the adjacent lane. Left or right hand versions of the sign should be used as required.

TW - 7. 2 sign components must never be erected separately.

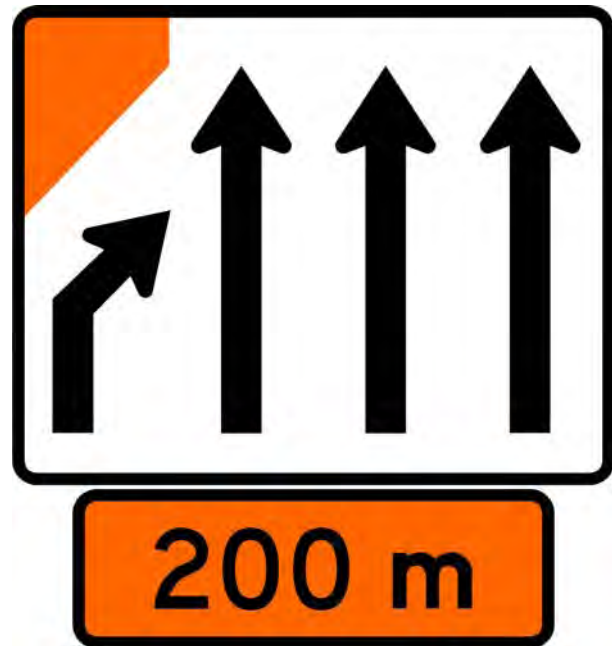
TW - 7.2 signs may only be erected by employees of controlling authority or, with the permission of the controlling authority by contractors or other authorised persons, to indicate lane closures on eight lane median divided two way roads (four lanes in each direction) and on four lane one way roads.

The signs shall be removed along with all other temporary signing as soon as the closed lane(s) have been reopened to traffic. This is the responsibility of the person in charge of the work party at the site.

Signs should be fitted to white painted, plain galvanised steel or plain aluminium stands. Under weather conditions likely to be experienced on all but very rare occasions the stands must be stable when the underside of a TW - 7.2B sign combination is located at least 1.0 m above the road surface.

Location: Normally, TW - 7.2 should be erected on both sides of the road and clear of the traffic lanes. Where this is not practicable they shall be located so as to be clearly visible to approaching drivers and not present an obstruction to traffic.

The NZTA CODE OF PRACTICE FOR TEMPORARY TRAFFIC MANAGEMENT specifies location and layout requirements for Temporary Warning signs. Special attention must be given to ensuring that the minimum clear visibility and spacing distances between the advance sign and the start of the road works site are achieved.



TW - 7.2B

For dimensions refer to <http://www.nzta.govt.nz/resources/traffic-control-devices-manual/sign-specifications/>

Policy: A TW - 8 LANE SHIFT - TWO LANE ONE WAY ROAD sign consists of two (2) angled ARROW symbols displayed side by side on a square TW sign panel. A supplementary sign displaying the distance to the lane shift is added to the TW - 8B sign to form a sign combination. The symbols indicate that traffic lanes on the road ahead have been shifted off from the normal road alignment to bypass a road works site. Left or right hand versions of the sign should be used as required.

TW - 8 sign components must never be erected separately.

TW - 8 signs may only be erected by employees of the controlling authority, or with permission of the controlling authority by contractors or other authorised persons, on six lane median divided two way roads (three lanes in each direction) and on three lane one way roads.

Signs should be fitted to white painted, plain galvanised steel or plain aluminium stands. Under weather conditions likely to be experienced on all but very rare occasions the stands must be stable when:

- the centre of a TW - 8A square sign is located at least 1.25 m above the road surface, or
- the underside of a TW - 8B sign combination is located at least 1.0 m above the road surface.

Location: Normally, TW - 8.1 should be erected on both sides of the road and clear of the traffic lanes. Where this is not practicable they shall be located so as to be clearly visible to approaching drivers and not present an obstruction to traffic.

The NZTA CODE OF PRACTICE FOR TEMPORARY TRAFFIC MANAGEMENT specifies location and layout requirements for Temporary Warning signs. Special attention must be given to ensuring that the minimum clear visibility and spacing distances between the advance sign and the start of the road works site are achieved.



TW - 8A



TW - 8B

Refer to Figures 5.2 and 5.4 for details of standard TW sign plates.

For dimensions refer to <http://www.nzta.govt.nz/resources/traffic-control-devices-manual/sign-specifications/>

Policy: A TW - 8.1 LANE SHIFT - THREE LANE ONE WAY ROAD sign combination consists of three (3) curved ARROW symbols displayed side by side on a special rectangular TW sign panel, and a supplementary sign displaying the distance to the lane shift. The symbols indicate that the traffic lanes ahead are shifted away from the normal road alignment. Left or right hand versions of the sign should be used as required.

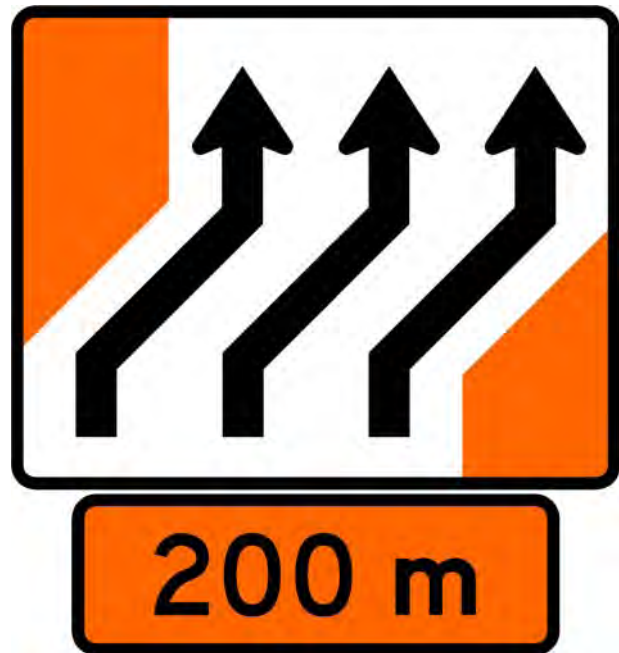
TW - 8.1 sign components must never be erected separately.

TW - 8.1 signs may only be erected by employees of the controlling authority, or with permission of the controlling authority by contractors or other authorised persons, on six lane median divided two way roads (three lanes in each direction) and on three lane one way roads.

Signs should be fitted to white painted, plain galvanised steel or plain aluminium stands. Under weather conditions likely to be experienced on all but very rare occasions the stands must be stable when the underside of a TW - 8.1B sign combination is located at least 1.0 m above the road surface.

Location: Normally, TW - 8.1 signs should be erected on both sides of the road and clear of the traffic lanes. Where this is not practicable they shall be located so as to be clearly visible to approaching drivers and not present an obstruction to traffic.

The NZTA CODE OF PRACTICE FOR TEMPORARY TRAFFIC MANAGEMENT specifies location and layout requirements for Temporary Warning signs. Special attention must be given to ensuring that the minimum clear visibility and spacing distances between the advance sign and the start of the road works site are achieved.



TW - 8.1

For dimensions refer to <http://www.nzta.govt.nz/resources/traffic-control-devices-manual/sign-specifications/>

Policy: A T W - 9 MERGING TRAFFIC - MAIN ROAD sign combination consists of a short angled ARROW symbol and two (2) vertical side by side ARROW symbols displayed on a standard square TW - B sign panel, and a supplementary sign displaying the distance to the merge. The symbols indicate that traffic entering from an on ramp will be merging into the main road traffic flow.

The sign will normally only be used on a motorway or expressway type roads where, due to road works such as a lane closure on the main road, an on ramp needs to be extended to join into the main road in a temporary manner.

TW - 9 signs may only be erected by employees of the controlling authority, or with permission of the controlling authority by contractors or other authorised persons, on multilane undivided two way roads, on multilane median divided two way roads (several lanes in each direction) and on multilane lane one way roads.

Signs should be fitted to white painted, plain galvanised steel or plain aluminium stands. Under weather conditions likely to be experienced on all but very rare occasions the stands must be stable when the underside of a TW - 9 sign combination is located at least 1.0 m above the road surface.

Location: Normally, TW - 9 signs should be erected on the left side of the main road in advance of an on ramp and clear of the traffic lanes. Where this is not practicable they shall be located so as to be clearly visible to approaching drivers and not present an obstruction to traffic.

The NZTA CODE OF PRACTICE FOR TEMPORARY TRAFFIC MANAGEMENT specifies location and layout requirements for Temporary Warning signs. Special attention must be given to ensuring that the minimum clear visibility and spacing distances between the advance sign and the start of the road works site are achieved.



TW - 9

Refer to Figures 5.2 and 5.4 for details of standard TW sign plates.

For dimensions refer to <http://www.nzta.govt.nz/resources/traffic-control-devices-manual/sign-specifications/>

Policy: TW - 10 MERGING TRAFFIC - SIDE ROAD sign combination consists of a short vertical ARROW symbol and two (2) angled side by side ARROW symbols displayed on a standard square TW - B sign panel, and a supplementary sign displaying the distance to the main road merge. The symbols indicate that onramp traffic will merge into the main road traffic flow.

The sign will normally only be used on a motorway or expressway type main roads where, due to road works such as a lane closure on the main road, an on ramp needs to be extended to join up with the main road traffic lanes in a temporary manner.

TW - 10 signs may only be erected by employees of the controlling authority, or with permission of the controlling authority by contractors or other authorised persons, on multilane lane undivided two way roads, on multilane median divided two way roads (several lanes in each direction) and on multilane lane one way roads.

Signs should be fitted to white painted, plain galvanised steel or plain aluminium stands. Under weather conditions likely to be experienced on all but very rare occasions the stands must be stable when the underside of a TW - 10 sign combination is located at least 1.0 m above the road surface.

Location: Normally, TW - 10 signs should be erected in advance of the on the left side of the on ramp and clear of the traffic lanes. Where this is not practicable they shall be located so as to be clearly visible to approaching drivers and not present an obstruction to traffic.

The NZTA CODE OF PRACTICE FOR TEMPORARY TRAFFIC MANAGEMENT specifies location and layout requirements for Temporary Warning signs. Special attention must be given to ensuring that the minimum clear visibility and spacing distances between the advance sign and the start of the road works site are achieved.



TW - 10

Refer to Figures 5.2 and 5.4 for details of standard TW sign plates.

For dimensions refer to <http://www.nzta.govt.nz/resources/traffic-control-devices-manual/sign-specifications/>

Policy: TW - 11 ADVANCE EXIT sign combination consists of a short angled ARROW symbol and two (2) straight vertical side by side ARROW symbols displayed on a standard square TW - B sign panel, and a supplementary sign displaying the distance to the exit. The symbols indicate that traffic can exit the main road at an off ramp.

The sign will normally only be used on a motorway or expressway type main roads where, due to road works such as a lane closure on the main road, an exit from the main road needs to be marked in a temporary manner.

TW - 11 signs may only be erected by employees of the controlling authority, or with permission of the controlling authority by contractors or other authorised persons on multilane lane undivided two way roads, on multilane median divided two way roads (several lanes in each direction) and on multilane lane one way roads.

Signs should be fitted to white painted, plain galvanised steel or plain aluminium stands. Under weather conditions likely to be experienced on all but very rare occasions the stands must be stable when the underside of a TW - 11 sign combination is located at least 1.0 m above the road surface.

Location: Normally TW - 11 signs should be erected on the left side of the road and clear of the traffic lanes. Where this is not practicable they shall be located so as to be clearly visible to approaching drivers and not present an obstruction to traffic.

The NZTA CODE OF PRACTICE FOR TEMPORARY TRAFFIC MANAGEMENT specifies location and layout requirements for Temporary Warning signs. Special attention must be given to ensuring that the minimum clear visibility and spacing distances between the advance sign and the start of the road works site are achieved.



TW - 11

Refer to Figures 5.2 and 5.4 for details of standard TW sign plates.

For dimensions refer to <http://www.nzta.govt.nz/resources/traffic-control-devices-manual/sign-specifications/>

Policy: TW - 12 EXIT DIRECTION sign consists of a short angled ARROW symbol and two (2) straight vertical side by side ARROW symbols displayed on a standard square TW - B sign panel, and a supplementary sign displaying the distance to the exit. The symbols indicate that traffic can turn off the main road at an exit facility, normally an off ramp.

The sign will normally only be used on a motorway or expressway type main roads when one of the main road lanes has been closed and an off ramp exit traffic lane has been extended to meet up with the remaining operational main road lanes.

TW - 11 signs may only be erected by employees of the controlling authority, or with permission of the controlling authority by contractors or other authorised persons, on multilane lane undivided two way roads, on multilane median divided two way roads (several lanes in each direction) and on multilane lane one way roads.

Signs should be fitted to white painted, plain galvanised steel or plain aluminium stands. Under weather conditions likely to be experienced on all but very rare occasions the stands must be stable when the underside of a TW - 11 sign combination is located at least 1.0 m above the road surface.

Signs should be fitted to white painted, unpainted galvanised steel or unpainted aluminium stands which provide stability under weather conditions likely to be experienced on all but very rare occasions with signs being located above the road surface by at least the following distances:

- 1.25 m to the centre of a TW - A diamond type sign, and
- 1.0 m to the underside of a TW - B square type sign or supplementary plate of a sign combination.

Location: Normally, TW - 5 signs should be erected clear of the traffic lanes. Where this is not practicable they shall be located so as to be clearly visible to approaching drivers and not present an obstruction to traffic.

The NZTA CODE OF PRACTICE FOR TEMPORARY TRAFFIC MANAGEMENT specifies location and layout requirements for Temporary Warning signs. Special attention must be given to ensuring that the minimum clear visibility and spacing distances between the advance sign and the start of the road works site are achieved.



TW - 12

Refer to Figure 5.2 for details of the standard TW - B sign plate.

For dimensions refer to <http://www.nzta.govt.nz/resources/traffic-control-devices-manual/sign-specifications/>



TW - 13



TW - 13.1

Refer to Figures 5.1 and 5.3 for details of standard TW sign plates.

For dimensions refer to <http://www.nzta.govt.nz/resources/traffic-control-devices-manual/sign-specifications/>

Policy: A TW - 13 sign combination consists of a ROAD NARROWS - LEFT OR RIGHT SIDE NARROWING symbol displayed on a TW - 13 sign plate and a supplementary sign ONE LANE. The alternative TW - 13.1 sign combination displays a ROAD NARROWS - BOTH SIDES NARROWING symbol and a supplementary sign ONE LANE. **These sign components must never be erected separately.**

TW - 13 (TW - 13.1) traffic direction signs may only be erected by employees of the controlling authority, or with the permission of the controlling authority, by contractors or other authorised persons. TW - 13 signs are only used on two lane two way roads where one direction of traffic must cross the centreline to facilitate road works and the road is effectively reduced to a single lane (there being no alternative temporary lanes provided). Where the single lane condition results from the narrowing of the shoulder side of both lanes then TW - 13.1 signs should be used.

TW - 13 (TW - 13.1) signs must be augmented with TW - 1 ROAD WORKS signs, an RG - 19 SINGLE LANE - GIVE WAY / RG - 20 SINGLE LANE - PRIORITY sign pair and, as necessary, with TW - 23 DETOUR, RG - 4 SPEED LIMIT - TEMPORARY signs and RP - 1.1 NO STOPPING AT ALL TIMES signs.

TW - 13 (TW - 13.1) signs are not to be used in situations where manual traffic control or temporary traffic signal control is employed.

The signs shall be removed along with all other temporary signing as soon as the normal two lane roadway has been re-opened to traffic. It is the responsibility of the person in charge of the work party at the site.

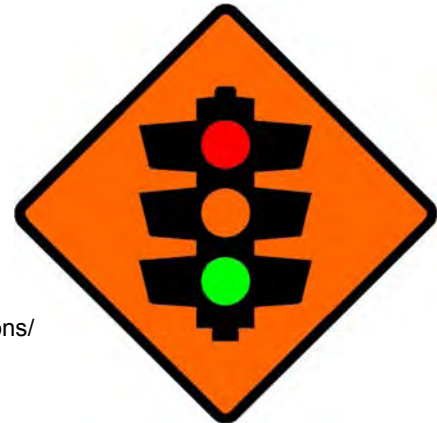
Signs should be fitted to white painted, plain galvanised steel or plain aluminium stands. Under weather conditions likely to be experienced on all but very rare occasions the stands must be stable when the centre of a TW - 13 (TW - 13.1) diamond sign is located at least 1.25 m above the road surface.

Location: Normally, TW - 13 (TW - 13.1) signs should be erected clear of the traffic lanes. Where this is not practicable they shall be located so as to be clearly visible to approaching drivers and not present an obstruction to traffic.

The NZTA CODE OF PRACTICE FOR TEMPORARY TRAFFIC MANAGEMENT specifies location and layout requirements for Temporary Warning signs. Special attention must be given to ensuring that the minimum clear visibility and spacing distances between the advance sign and the start of the road works site are achieved.

For dimensions refer to

<http://www.nzta.govt.nz/resources/traffic-control-devices-manual/sign-specifications/>



TW -14A

SYMBOL : *black with reflectorised red, orange and green inserts*



TW - 14B

Refer to Figures 5.1 and 5.2 for details of standard TW sign plates.

Policy: TW - 14 TRAFFIC SIGNALS - TEMPORARY traffic control signs may only be erected by employees of the controlling authority, or with the permission of the controlling authority, by contractors or other authorised persons. TW - 14 signs are normally only used at road works sites on two lane two way roads to provide advance warning of temporary traffic signals that are being used to control traffic in situations where only one traffic lane is available.

TW - 14 signs must be augmented with TW - 1 ROAD WORKS signs and RG - 4 SPEED LIMIT- TEMPORARY signs and, as necessary RP - 1.1 NO STOPPING AT ALL TIMES signs.

TW - 13 or TW - 13.1 ONE LANE signs are not to be used when traffic is being controlled by temporary traffic signals.

The signs shall be removed along with all other temporary signing as soon as the normal two lane roadway has been re-opened to traffic. This is the responsibility of the person in charge of the work party at the site.

Signs should be fitted to white painted, plain galvanised steel or plain aluminium stands. Under weather conditions likely to be experienced on all but very rare occasions the stands must be stable when:

- the centre of a TW - 14A diamond sign is located at least 1.25 m above the road surface, or
- the underside of a TW - 14B sign is located at least 1.0 m above the road surface.

Location: Normally, TW - 14 signs should be erected clear of the traffic lanes. Where this is not practicable they shall be located so as to be clearly visible to approaching drivers and not present an obstruction to traffic.

The NZTA CODE OF PRACTICE FOR TEMPORARY TRAFFIC MANAGEMENT specifies location and layout requirements for Temporary Warning signs. Special attention must be given to ensuring that the minimum clear visibility and spacing distances between the advance sign and the start of the road works site are achieved.

Policy: TW - 15 sign consists of a MANUALTRAFFIC CONTROLLER symbol displayed on a standard TW sign plate.

TW - 15 traffic control signs may only be erected by employees of the controlling authority or, with the permission of the controlling authority, by contractors or other authorised persons.

The signs are normally only used at road works sites on two lane two way roads where there is only one traffic lane available, to provide advance warning of manual traffic control using TW - 35 STOP / GO paddles.

TW - 15 signs must be augmented with TW - 1 ROAD WORKS signs and RG - 4 SPEED LIMIT - TEMPORARY signs and, as necessary, RP-1.1 NO STOPPING AT ALL TIMES signs.

TW - 13 or TW - 13.1 ONE LANE signs must not be used when traffic is being manually controlled.

The signs shall be removed along with all other temporary signing as soon as the normal two lane roadway has been re-opened to traffic. This is the responsibility of the person in charge of the work party at the site.

Signs should be fitted to white painted, plain galvanised steel or plain aluminium stands. Under weather conditions likely to be experienced on all but very rare occasions the stands must be stable when:

- the centre of a TW - 15A diamond sign is located at least 1.25 m above the road surface, or
- the underside of a TW - 15B sign is located at least 1.0 m above the road surface.

Location: Normally, TW - 15 signs should be erected clear of the traffic lanes. Where this is not practicable they shall be located so as to be clearly visible to approaching drivers and not present an obstruction to traffic.

The NZTA CODE OF PRACTICE FOR TEMPORARY TRAFFIC MANAGEMENT specifies location and layout requirements for Temporary Warning signs. Special attention must be given to ensuring that the minimum clear visibility and spacing distances between the advance sign and the start of the road works site are achieved.



TW - 15A



TW - 15B

Refer to Figures 5.1 and 5.2 for details of standard TW sign plates.

For dimensions refer to <http://www.nzta.govt.nz/resources/traffic-control-devices-manual/sign-specifications/>

Policy: A TW - 15.1 sign combination consists of a MANUAL TRAFFIC CONTROLLER symbol displayed on a standard TW diamond sign plate and a supplementary sign PLEASE STOP ON REQUEST (the stand alone TW - 18 sign).

A TW - 15.1 sign may be used instead of a TW - 15 sign in situations where the additional traffic direction message is considered necessary for a safe and efficient manual traffic control operation.

TW - 15.1 traffic control signs may only be erected by employees of the controlling authority or, with the permission of the controlling authority, by contractors or other authorised persons. TW - 15.1 signs are normally only used at road works sites on two lane two way roads where there is only one traffic lane available, to provide advance warning of manual traffic control using TW - 35 STOP / GO paddles.

TW - 15.1 signs must be augmented with TW - 1 ROAD WORKS signs and RG - 4 SPEED LIMIT - TEMPORARY signs and, as necessary, RP-1.1 NO STOPPING AT ALL TIMES signs.

TW - 13 or TW - 13.1 ONE LANE signs must not be used when traffic is being manually controlled.

The signs shall be removed along with all other temporary signing as soon as the normal two lane roadway has been re-opened to traffic. This is the responsibility of the person in charge of the work party at the site.

Signs should be fitted to white painted, plain galvanised steel or plain aluminium stands. Under weather conditions likely to be experienced on all but very rare occasions the stands must be stable when:

- the centre of a TW - 15.1A diamond sign is located at least 1.25 m above the road surface, or
- the underside of a TW - 15.1B sign combination is located at least 1.0 m above the road surface.

Location: Normally, TW - 15.1 signs should be erected clear of the traffic lanes. Where this is not practicable they shall be located so as to be clearly visible to approaching drivers and not present an obstruction to traffic.

The NZTA CODE OF PRACTICE FOR TEMPORARY TRAFFIC MANAGEMENT specifies location and layout requirements for Temporary Warning signs. Special attention must be given to ensuring that the minimum clear visibility and spacing distances between the advance sign and the start of the road works site are achieved.



TW - 15.1A



TW - 15.1B

Refer to Figures 5.1, 5.2, 5.3 and 5.4 for details of standard TW sign plates.

For dimensions refer to <http://www.nzta.govt.nz/resources/traffic-control-devices-manual/sign-specifications/>

For dimensions refer to

<http://www.nzta.govt.nz/resources/traffic-control-devices-manual/sign-specifications/>



TW - 16A



TW - 16B

Policy: TW - 16 WORKS END signs may only be erected by employees of the controlling authority or with the permission of the controlling authority by contractors or other authorised persons, to indicate the end of a section of road that has been signed to give advance warning of road works activities.

A controlling authority may approve the erection of a TW - 17 THANK YOU sign immediately below a TW - 16 sign to form a sign combination, when the additional message is considered desirable.

TW - 16 signs are only used in conjunction with TW - 1 advance warning signs, ie.

TW - 1 ROAD WORKS,

TW - 1B.1 ROAD WORKS - 1km,

TW - 1B.2 ROAD WORKS - 1 km,

DELAYS POSSIBLE,

TW - 1B.3 ROAD WORKS - '0 km/h AHEAD,

TW - 1.1 ROAD WORKS - NEXT " " km,

TW - 1.2 ROAD WORKS - WET TAR,

TW - 1.3 ROAD WORKS - GRADER, etc

(except where a mobile sign is used),

TW - 1.4 ROAD WORKS - ROAD MARKING,

TW - 1.5 ROAD WORKS - ON SIDE ROAD,

TW - 1.6 ROAD WORKS - SHOULDER CLOSED, and

TW - 1.7 ROAD WORKS SURVEYING.

The signs shall be removed along with all other temporary signing as soon as the need for the warning has ceased. This is the responsibility of the person in charge of the work party at the site.

Signs should be fitted to white painted, plain galvanised steel or plain aluminium stands. Under weather conditions likely to be experienced on all but very rare occasions the stands must be stable when:

- the underside of a TW - 16A sign, or a TW - 16A / TW - 17A sign combination, is located at least 0.5 m above the road surface, or
- the underside of a TW - 16B sign, or a TW - 16B / TW - 17B sign combination, is located at least 1.0 m above the road surface.

Refer to Figures 5.3 and 5.4 for details of standard TW sign plates.

Location: Normally, TW - 16 signs should be erected clear of the traffic lanes. Where this is not practicable they shall be located so as to be clearly visible to approaching drivers and not present an obstruction to traffic. The signs should normally be erected approximately 20 m beyond the end of the road work activity.

The NZTA CODE OF PRACTICE FOR TEMPORARY TRAFFIC MANAGEMENT specifies location and layout requirements for Temporary Warning signs. Special attention must be given to ensuring that the minimum clear visibility and spacing distances between the advance sign and the start of the road works site are achieved.

WORKS END

TW - 16

(used with TW - 1 ROAD WORKS signs)



TW - 17A

For dimensions refer to

<http://www.nzta.govt.nz/resources/traffic-control-devices-manual/sign-specifications/>



TW - 17B

Policy: TW - 17 THANK YOU signs may only be erected by employees of the controlling authority, or with the permission of the controlling authority by contractors or other authorised persons, to indicate the end of a section of road that has been signed to give advance warning of temporary hazards other than road works activities on, or near, the road.

TW - 17 signs should be used in conjunction with the following signs:

TW - 2.2 OTHER HAZARD - WASHOUT,
TW - 2.3 OTHER HAZARD - LINEMEN,
TW - 2.4 OTHER HAZARD - BLASTING,
TW - 2.5 OTHER HAZARD - TREE FELLING,
TW - 2.6 OTHER HAZARD - LOGGING TRUCKS,
TW - 2.7 OTHER HAZARD - TRUCKS CROSSING,
TW - 2.8 OTHER HAZARD - NO ROAD MARKING,
TW - 2.9 OTHER HAZARD - SIGNALS CHANGED,
TW - 2.10 OTHER HAZARD - SIGNALS NOT WORKING,
TW - 2.11 OTHER HAZARD - NEW ROAD LAYOUT,
TW - 2.12 OTHER HAZARD - TRAFFIC SURVEY,
TW - 2.13 OTHER HAZARD - CYCLE RACE,
TW - 2.14 OTHER HAZARD - ACCIDENT,
TW - 2.15 OTHER HAZARD - FIRE,
TW - 3 SLIPS,
TW - 4 SLIPPERY SURFACE,
TW - 4.1 SLIPPERY SURFACE - ICE / GRIT,
TW - 5 GRAVEL SURFACE,
TW - 5.1 GRAVEL SURFACE- NEW SEAL,
TW - 5.2 GRAVEL SURFACE- SEAL REPAIRS, and
TW - 6 (TW - 6.1) STOCK - CATTLE (SHEEP).

The controlling authority may also approve the use of a TW - 17 THANK YOU sign immediately below a TW - 16 sign, to form a sign combination when the additional message is considered desirable.

The signs shall be removed along with all other temporary signing as soon as the need for the warning has ceased.

Refer to Figures 5.3 and 5.4 for details of standard TW sign plates.

Signs should be fitted to white painted, plain galvanised steel or plain aluminium stands. Under weather conditions likely to be experienced on all but very rare occasions the stands must be stable when:

- the underside of a TW - 17A sign is located at least 0.5 m above the road surface, or
- the underside of a TW - 17B sign is located at least 1.0 m above the road surface.

Location: Normally, TW - 17 signs are erected clear of the traffic lanes but where this is not practicable they shall be located so as to be clearly visible to approaching drivers and not present an obstruction to traffic. The signs are normally erected on the left hand side of the road approximately 20 m beyond the end of the hazard zone.

The NZTA CODE OF PRACTICE FOR TEMPORARY TRAFFIC MANAGEMENT specifies location and layout requirements for Temporary Warning signs. Special attention must be given to ensuring that the minimum clear visibility and spacing distances between the advance sign and the start of the road works site are achieved.

THANK YOU
(used with TW - 2, TW - 3, TW - 4, TW - 5 and TW - 6 signs) TW - 17



For dimensions refer to

<http://www.nzta.govt.nz/resources/traffic-control-devices-manual/sign-specifications/>

TW - 18A



TW - 18B

Policy: A TW - 18 sign consists of the words PLEASE STOP ON REQUEST displayed on a standard TW supplementary sign plate.

TW - 18 signs are used as:

- (a) stand-alone signs which must be erected in advance of TW - 2.12 TRAFFIC SURVEY signs, and
- (b) supplementary plates for TW - 15.1 MANUAL TRAFFIC CONTROL sign combinations.

A stand-alone TW - 18 traffic direction sign may only be erected for the duration of traffic surveydriver interviews by employees of the controlling authority, or with permission of the controlling authority by contractors or other authorised persons, on the approaches to roadside traffic survey sites.

Stand-alone TW - 18 signs should be fitted to white painted, plain galvanised steel or plain aluminium stands. Under weather conditions likely to be experienced on all but very rare occasions the stands must be stable when:

- the underside a TW - 18A sign is located at least 0.5 m above the road surface, or
- the underside of a TW - 18B sign is located at least 1.0 m above the road surface.

Location: Normally, TW - 18 signs should be erected clear of the traffic lanes. Where this is not practicable they shall be located so as to be clearly visible to approaching drivers and not present an obstruction to traffic.

The NZTA CODE OF PRACTICE FOR TEMPORARY TRAFFIC MANAGEMENT specifies location and layout requirements for Temporary Warning signs. Special attention must be given to ensuring that the minimum clear visibility and spacing distances between the TW - 2.12 advance sign and the TW - 18 traffic direction sign.



TW - 19A

For dimensions refer to

<http://www.nzta.govt.nz/resources/traffic-control-devices-manual/sign-specifications/>



TW - 19B

Policy: A TW - 19 sign consists of the words DRY YOUR BRAKES displayed on a standard TW supplementary sign plate

TW -19 traffic direction signs may only be erected by employees of the controlling authority, or with the permission of the controlling authority by contractors or other authorised persons, to indicate the end of a section of road that has been signed with TW - 2.1 FLOODING signs.

The signs shall be removed along with the TW - 21 advance warning signs as soon as the surface flooding is no longer a hazard.

Signs should be fitted to white painted, plain galvanised steel or plain aluminium stands. Under weather conditions likely to be experienced on all but very rare occasions the stands must be stable when:

- the underside a TW - 19A sign is located at least 0.5 m above the road surface, or
- the underside of a TW - 19B sign is located at least 1.0 m above the road surface.

Location: Normally, TW - 19 signs should be erected approximately 20 m past flooded section of road and clear of the traffic lanes. Where this is not practicable they shall be located so as to be clearly visible to approaching drivers and not present an obstruction to traffic.

The NZTA CODE OF PRACTICE FOR TEMPORARY TRAFFIC MANAGEMENT specifies location and layout requirements for Temporary Warning signs. Special attention must be given to ensuring that the minimum clear visibility and spacing distances between the advance sign and the start of the road works site are achieved.

Policy: A T W - 20 sign consists of the words ROAD CLOSED AHEAD or EXT CLOSED AHEAD displayed on a special TW sign plate.

TW - 20 signs may only be erected by employees of the controlling authority, or with the permission of the controlling authority by contractors or other authorised persons, to indicate that the road is closed ahead. In normal circumstances an alternative route or detour will also be provided.

Signs should be fitted to white painted, plain galvanised steel or plain aluminium stands. Under weather conditions likely to be experienced on all but very rare occasions the stands must be stable when:

- the underside of a TW - 20A sign is located at least 0.5 m to above the road surface, and
- the underside of a TW - 20B sign is located at least 1.0 m above the road surface.

Location: Normally, TW - 20 signs should be erected clear of the traffic lanes. Where this is not practicable they shall be located so as to be clearly visible to approaching drivers and not present an obstruction to traffic.

The NZTA CODE OF PRACTICE FOR TEMPORARY TRAFFIC MANAGEMENT specifies location and layout requirements for Temporary Warning signs. Special attention must be given to ensuring that the minimum clear visibility and spacing distances between the advance sign and the start of the road works site are achieved.



TW - 20A



TW - 20B

For dimensions refer to <http://www.nzta.govt.nz/resources/traffic-control-devices-manual/sign-specifications/>

Policy: A TW - 21 sign consists of the words DETOUR AHEAD FOLLOW "symbol" displayed on a special TW sign plate.

TW - 21 traffic direction signs may only be erected by employees of the controlling authority, or with the permission of the controlling authority by contractors or other authorised persons, when the road ahead is closed and a detour route, which is indicated with signs displaying a distinctive route marker symbol, is in use.

The route marker symbol should be a simple shape and large enough to be easily recognised by drivers. Three suitable symbol shapes are detailed below.

Signs should be fitted to white painted, plain galvanised steel or plain aluminium stands. Under weather conditions likely to be experienced on all but very rare occasions the stands must be stable when:

- the underside of a TW - 21A sign is located at least 0.5 m above the road surface, and
- the underside of a TW - 21B sign is located at least 1.0 m above the road surface.

Location: Normally, TW - 21 signs should be erected clear of the traffic lanes. Where this is not practicable they shall be located so as to be clearly visible to approaching drivers and not present an obstruction to traffic.

The NZTA CODE OF PRACTICE FOR TEMPORARY TRAFFIC MANAGEMENT specifies location and layout requirements for Temporary Warning signs. Special attention must be given to ensuring that the minimum clear visibility and spacing distances between the advance sign and the start of the road works site are achieved.



TW - 21A



TW - 21B

For dimensions refer to <http://www.nzta.govt.nz/resources/traffic-control-devices-manual/sign-specifications/>



TW - 22 (R)

TW - 22 (L) is a mirror image of above

For dimensions refer to

<http://www.nzta.govt.nz/resources/traffic-control-devices-manual/sign-specifications/>



TW - 22.1 (L)

TW - 22.1 (R) is a mirror image of above

Policy: TW - 22 traffic direction signs display a route marker symbol and a traffic directional arrow on a standard TW / C sign plate.

TW - 22 signs may only be erected by employees of the controlling authority, or with the permission of the controlling authority by contractors or other authorised persons, to indicate the road is closed ahead when an alternative route or detour has been provided.

The signs must be erected at each change of direction of the detour route. Additional signs should also be erected at frequent intervals along the route between changes of direction, and at any intersection where the correct direction to follow might not be obvious, to provide confirmation that drivers are on the detour route.

Signs should be fitted to white painted, plain galvanised steel or plain aluminium stands. Under weather conditions likely to be experienced on all but very rare occasions the stands must be stable when the underside of the sign is located at least 1.0 m above the road surface.

Location: Normally, TW - 22 signs should be erected clear of the traffic lanes. Where this is not practicable they shall be located so as to be clearly visible to approaching drivers and not present an obstruction to traffic.

The NZTA CODE OF PRACTICE FOR TEMPORARY TRAFFIC MANAGEMENT specifies location and layout requirements for Temporary Warning signs. Special attention must be given to ensuring that the minimum clear visibility and spacing distances between the advance sign and the start of the road works site are achieved.



TW - 22.2 (R)

TW - 22.2 (L) is a mirror image of above



TW - 23

For dimensions refer to <http://www.nzta.govt.nz/resources/traffic-control-devices-manual/sign-specifications/>

Policy: A TW - 23 sign displays the words DETOUR ENDS on a standard TW / D supplementary sign plate.

TW - 23 signs may only be erected by employees of the controlling authority, or with the permission of the controlling authority by contractors or other authorised persons, to indicate the end of a temporary detour route.

Signs should be fitted to white painted, plain galvanised steel or plain aluminium stands. Under weather conditions likely to be experienced on all but very rare occasions the stands must be stable when the underside of the sign is located at least 1.0 m above the road surface.

Location: Normally, TW - 23 signs should be erected clear of the traffic lanes. Where this is not practicable they shall be located so as to be clearly visible to approaching drivers and not present an obstruction to traffic.

The NZTA CODE OF PRACTICE FOR TEMPORARY TRAFFIC MANAGEMENT specifies location and layout requirements for Temporary Warning signs. Special attention must be given to ensuring that the minimum clear visibility and spacing distances between the advance sign and the start of the road works site are achieved.



For dimensions refer to

TW - 24

<http://www.nzta.govt.nz/resources/traffic-control-devices-manual/sign-specifications/>



TW - 24.1

LEGEND : *black*
BACKGROUND : *Class 1 reflectorised orange*
BORDER : *black*

Policy: TW - 24 traffic direction signs may only be erected by employees of the controlling authority or with the permission of the controlling authority by contractors or other authorised persons to augment other temporary road works signs at work sites where the road pavement is in the process of reconstruction by means of lime or cement stabilisation and where because of rain following the stabilisation process, traffic is required to traverse the saturated pavement resulting in vehicles becoming contaminated with lime or cement splashes. The signs which would be worded appropriately "LIME" or "CEMENT" would not be required under normal dry working conditions.

The signs must be removed as soon as the danger of splashing has passed. This is the responsibility of the person in charge of the work party at the site.

Signs should be fitted to white painted, plain galvanised steel or plain aluminium stands. Under weather conditions likely to be experienced on all but very rare occasions the stands must be stable when the underside of the sign is located at least 0.5 m above the road surface.

Location: Normally, TW - 24 signs should be erected clear of the traffic lanes. Where this is not practicable they shall be located so as to be clearly visible to approaching drivers and not present an obstruction to traffic.

The NZTA CODE OF PRACTICE FOR TEMPORARY TRAFFIC MANAGEMENT specifies location and layout requirements for Temporary Warning signs. Special attention must be given to ensuring that the minimum clear visibility and spacing distances between the advance sign and the start of the road works site are achieved.

TW - 24
TW - 24.1

CEMENT (LIME) SPLASHES
WASH CAR TODAY

**TW - 25**

LEGEND	:	<i>black</i>
BACKGROUND	:	<i>reflectorised fluorescent orange</i>
BORDER	:	<i>black</i>

For dimensions refer to <http://www.nzta.govt.nz/resources/traffic-control-devices-manual/sign-specifications/>

Policy: A TW - 25 vehicle mounted traffic direction sign consists of the words PILOT CAR FOLLOW ME displayed on a TW / B sign plate, with a reflectorised fluorescent orange background.

TW - 25 signs may only be used in conjunction with manual traffic controllers, by contractors or other authorised persons, when, there is a need to lead traffic through a work site at a desired speed.

Location: TW - 25 signs must be attached to the rear, or roof, of the pilot vehicle used lead traffic through the work site, and in such a position as to be clearly visible to following drivers.

The NZTA CODE OF PRACTICE FOR TEMPORARY TRAFFIC MANAGEMENT specifies location and layout requirements for Temporary Warning signs and manual traffic control procedures. Special attention must be given to ensuring that the minimum clear visibility and spacing distances between the advance sign and the start of the road works site are achieved.



TW - 26

LEGEND	:	<i>black</i>
BACKGROUND	:	<i>reflectorised fluorescent orange</i>
BORDER	:	<i>black</i>

For dimensions refer to <http://www.nzta.govt.nz/resources/traffic-control-devices-manual/sign-specifications/>

Policy: A TW - 26 vehicle mounted sign consists of the words ROAD WORKS displayed on a TW / D sign plate.

TW - 26 signs must be used in conjunction with vehicle mounted flashing yellow lights.

The signs may only be used by employees of the controlling authority, or with the permission of the controlling authority by contractors or other authorised persons, when carrying out mobile road works operations.

Location: TW - 26 signs must be mounted on the front of the lead pilot vehicle of all mobile road works operation, and in such a position as to be clearly visible to approaching drivers.

The NZTA CODE OF PRACTICE FOR TEMPORARY TRAFFIC MANAGEMENT specifies the vehicle requirements and sign location/layout requirements for temporary mobile operations.

**TW - 27**

LEGEND	:	<i>black</i>
BACKGROUND	:	<i>reflectorised fluorescent orange</i>
BORDER	:	<i>black</i>

For dimensions refer to <http://www.nzta.govt.nz/resources/traffic-control-devices-manual/sign-specifications/>

Policy: A TW - 27 vehicle mounted sign consists of the words ROAD INSPECTION displayed on a special TW sign plate.

TW - 27 signs must be used in conjunction with vehicle mounted flashing yellow lights.

The signs may only be used by employees of the controlling authority, or with the permission of the controlling authority by contractors or other authorised persons, while carrying out a temporary mobile operation such as a drive over road inspection or a similar type of road condition survey.

Location: TW - 27 signs must be attached to the rear of the inspection vehicle and in such a position as to be clearly visible to approaching drivers.

The NZTA CODE OF PRACTICE FOR TEMPORARY TRAFFIC MANAGEMENT specifies the vehicle requirements and sign location/layout requirements for temporary mobile operations.

Policy: TW - 28 advance warning signs contain a symbol indicating the location of the work site access and the words SITE ACCESS 100 m displayed on a TW - B size sign plate.

A TW - 28 sign may be erected to indicate to give advance warning of the approved access point to a work site located adjacent to the road, when the site access is directly off a live traffic lane on that road.

The signs must be covered or removed overnight and whenever work site activities are suspended for more than four (4) hours.

Signs should be fitted to white painted, plain galvanised steel or plain aluminium stands. Under weather conditions likely to be experienced on all but very rare occasions the stands must be stable when the underside of the sign is located at least 1.0 m above the road surface.

Location: Normally, TW - 28 signs should be erected clear of the traffic lanes. Where this is not practicable they shall be located so as to be clearly visible to approaching drivers and not present an obstruction to traffic.

The NZTA CODE OF PRACTICE FOR TEMPORARY TRAFFIC MANAGEMENT specifies location and layout requirements for Temporary Warning signs. Special attention must be given to ensuring that the minimum clear visibility and spacing distances between the advance sign and the start of the road works site are achieved.



TW - 28 (L)



TW - 28 (R)

SIGN PLATE	:	TW - B (both signs)
BACKGROUND	:	Class 1 reflectorised orange
BORDER	:	black
LEGEND and SYMBOL	:	black

For dimensions refer to <http://www.nzta.govt.nz/resources/traffic-control-devices-manual/sign-specifications/>

Policy: TW - 29 site access direction signs contain a symbol indicating the direction of the work site access, and the words SITE ACCESS, displayed on a TW - B size sign plate.

A TW - 29 signs may be erected at the approved access point to a work site located adjacent to the road, when the site access is directly off a live traffic lane on that road.

The signs must be covered or removed overnight and whenever work site activities are suspended for more than four (4) hours.

Signs should be fitted to white painted, plain galvanised steel or plain aluminium stands. Under weather conditions likely to be experienced on all but very rare occasions the stands must be stable when the underside of the sign is located at least 1.0 m above the road surface.

Location: Normally, TW - 29 signs should be erected clear of the traffic lanes. Where this is not practicable they shall be located so as to be clearly visible to approaching drivers and not present an obstruction to traffic.

The NZTA CODE OF PRACTICE FOR TEMPORARY TRAFFIC MANAGEMENT specifies location and layout requirements for Temporary Warning signs. Special attention must be given to ensuring that the minimum clear visibility and spacing distances between the advance sign and the start of the road works site are achieved.



TW - 29 (L)



TW - 29 (R)

SIGN PLATE	:	<i>TW - B (both signs)</i>
BACKGROUND	:	<i>Class 1 reflectorised orange</i>
LEGEND and SYMBOL	:	<i>black</i>
LETTERS	:	<i>160 Series D</i>
ARROW	:	<i>Type A</i>

For dimensions refer to <http://www.nzta.govt.nz/resources/traffic-control-devices-manual/sign-specifications/>



TW - 30

LEGEND	:	<i>black</i>
BACKGROUND	:	<i>Class 1 reflectorised orange</i>
BORDER	:	<i>black</i>

For dimensions refer to <http://www.nzta.govt.nz/resources/traffic-control-devices-manual/sign-specifications/>

Policy: TW - 30 pedestrian direction signs contain the words CROSSING CLOSED PLEASE USE ALTERNATIVE CROSSING displayed on a special TW sign plate.

TW - 30 signs should be erected where a formal pedestrian crossing cannot be used because of road works or some other temporary type work activity in the vicinity of the crossing.

TW - 32 signs must be used in conjunction with TW - 30 signs to direct pedestrians to the alternative pedestrian crossing.

Signs should be fitted to white painted, plain galvanised steel or plain aluminium stands. Under weather conditions likely to be experienced on all but very rare occasions the stands must be stable when the underside of the sign is located at least 1.0 m above the road or footpath surface.

Location: TW - 30 signs must be erected on the footpath in advance of the closed crossing, and where they are clearly visible to approaching pedestrians. The safe route to the alternative crossing must be clearly indicated and signed from this point.

Refer to the NZTA CODE OF PRACTICE FOR TEMPORARY TRAFFIC MANAGEMENT for further details of the location and layout requirements for temporary pedestrian direction signs.

**TW - 31**

LEGEND	:	<i>black</i>
BACKGROUND	:	<i>Class 1 reflectorised orange</i>
BORDER	:	<i>black</i>

For dimensions refer to <http://www.nzta.govt.nz/resources/traffic-control-devices-manual/sign-specifications/>

Policy: TW - 31 pedestrian direction signs contain the words FOOTPATH CLOSED PLEASE USE OTHER SIDE displayed on a special TW sign plate.

TW - 31 signs should be erected where a formal footpath cannot be used because of road works or some other temporary work site activity in the vicinity and there is an alternative footpath on the otherside of the road. Their use is however subject to the restrictions listed below.

TW - 31 signs must not be used on:

- ***roads with a posted speed limit of greater than 65 km/h, and***
- ***Level 2 and 3 roads.***

Signs should be fitted to white painted, plain galvanised steel or plain aluminium stands. Under weather conditions likely to be experienced on all but very rare occasions the stands must be stable when the underside of the sign is located at least 1.0 m above the road or footpath surface.

Location: TW - 31 signs must be erected on the footpath in advance of the closed section of footpath, where they are clearly visible to approaching pedestrians and it is safe to cross the road.

The NZTA CODE OF PRACTICE FOR TEMPORARY TRAFFIC MANAGEMENT specifies location and layout requirements for Temporary Warning signs. Special attention must be given to ensuring that the minimum clear visibility and spacing distances between the advance sign and the start of the road works site are achieved.



TW - 32P (L)

(TW - 32P (R) is a mirror image of above)

Policy: TW - 32 pedestrian / cyclist direction signs display a standard pedestrian or cyclists symbol and an arrow direction indicator on a standard TW / B sign plate. Left or right hand versions of the signs should be used as required

TW - 32 signs should be erected to direct pedestrians or cyclists to a temporary route or formal crossing point, and indicate the alignment of the temporary route when the normal facility is not useable because of road works or some other temporary work activity.

TW - 32P signs must be erected when TW - 30 CROSSING CLOSED PLEASE USE ALTERNATIVE CROSSING signs are erected, to direct pedestrians to the alternative crossing.

Signs should be fitted to white painted, plain galvanised steel or plain aluminium stands. Under weather conditions likely to be experienced on all but very rare occasions the stands must be stable when the underside of the sign is located at least 1.0 m above the footpath surface.

Location: TW - 32 signs should be erected clear of the temporary route and where they will be clearly visible to approaching pedestrians or cyclists. Signs should be provided at each change of temporary route direction and at any point where the alignment of the temporary route might not be obvious to pedestrians or cyclists.

The NZTA CODE OF PRACTICE FOR TEMPORARY TRAFFIC MANAGEMENT specifies location and layout requirements for Temporary Warning signs. Special attention must be given to ensuring that the minimum clear visibility and spacing distances between the advance sign and the start of the road works site are achieved.



TW - 32.1P (R)

(TW - 32.1P (L) is a mirror image of above)



TW - 32.2P (L)

(TW - 32.2P (R) is a mirror image of above)

BACKGROUND : Class 1 reflectorised orange
BORDER : black
SYMBOLS : black

For dimensions refer to <http://www.nzta.govt.nz/resources/traffic-control-devices-manual/sign-specifications/>

**TW - 32C (L)**

(TW - 32C (R) is a mirror image of above)

**TW - 32.1C (R)**

(TW - 32.1C (L) is a mirror image of above)

**TW - 32.2C (L)**

(TW - 32.2C (R) is a mirror image of above)

BACKGROUND : Class 1 reflectorised
orange
BORDER : black
SYMBOLS : black

For dimensions refer to <http://www.nzta.govt.nz/resources/traffic-control-devices-manual/sign-specifications/>



LEGEND : *reflectorised white*
BACKGROUND : *reflectorised red*
BORDER : *reflectorised white*



LEGEND : *reflectorised white*
BACKGROUND : *reflectorised green*
BORDER : *reflectorised whi*

For dimensions refer to <http://www.nzta.govt.nz/resources/traffic-control-devices-manual/sign-specifications/>

Policy: TW - 33 STOP / GO traffic control paddles are to be used by properly trained personnel to control traffic at work sites on two lane two way roads where only one traffic lane is available and where the use of temporary traffic signals is either unwarranted or inappropriate.

TW - 15 MANUAL TRAFFIC CONTROL and RG - 4 SPEED LIMIT - TEMPORARY signs must be used in conjunction with traffic controllers using TW - 33 STOP / GO traffic control paddles.

The NZTA CODE OF PRACTICE FOR TEMPORARY TRAFFIC MANAGEMENT specifies location and layout requirements for Temporary Warning signs and the operating practices to be followed for manual traffic control. This code should be strictly followed.

TW - 34 (R)



TW - 34 (L)

For dimensions refer to <http://www.nzta.govt.nz/resources/traffic-control-devices-manual/sign-specifications/>

Policy: A TW - 34 vehicle mounted sign combination consists of the words PASS WITH CARE displayed on a standard TW supplementary sign plate and an RG - 34 KEEP RIGHT sign or an RG - 17 KEEP LEFT sign.

TW - 34 signs may be substituted for the relevant TW signs required to be mounted on the rear of shadow and work vehicles involved in temporary mobile operations. The RG - 34 or RG - 17 sign may be omitted when the vehicle is fitted with an arrow board.

The signs may only be used by employees of the controlling authority, or with the permission of the controlling authority by contractors or other authorised persons, while carrying out a temporary mobile operation.

Location: TW - 34 signs must be attached to the rear of shadow and work vehicles in such a position as to be clearly visible to approaching drivers.

The NZTA CODE OF PRACTICE FOR TEMPORARY TRAFFIC MANAGEMENT specifies the vehicle requirements and sign location/layout requirements for temporary mobile operations.



TW - 35A

For dimensions refer to <http://www.nzta.govt.nz/resources/traffic-control-devices-manual/sign-specifications/>

Policy: A TW - 35 sign consists of two arrow symbols (as detailed) displayed on a standard TW sign plate.

TW - 35 DIVERGE signs may only be erected by employees of the controlling authority, or with the permission of the controlling authority, by contractors or other authorised persons, within road work sites where traffic lanes in the same direction are required to pass either side of a hazard.

NOTE: A TW - 35 signs **MUST never be used for a centre lane closure on a three-lane one-way carriageway.**

The signs shall be removed along with all other temporary signing as soon as the hazard has been removed. This is the responsibility of the person in charge of the work party at the site.

Signs should be fitted to white painted, plain galvanised steel or plain aluminium stands. Under weather conditions likely to be experienced on all but very rare occasions the stands must be stable when:

- the centre of a TW - 35A diamond sign is located at least 1.25 m above the road surface, or
- the underside of a TW - 35B sign is located at least 1.0 m above the road surface.

Location: Normally, TW - 35 signs should be erected immediately in front of the hazard and clear of the traffic lanes. Where this is not practicable they shall be located so as to be clearly visible to approaching drivers and not present an obstruction to traffic.

The NZTA CODE OF PRACTICE FOR TEMPORARY TRAFFIC MANAGEMENT specifies location and layout requirements for Temporary Warning signs. Special attention must be given to ensuring that the minimum clear visibility and spacing distances between the advance sign and the start of the road works site are achieved.



TW - 35B

Refer to Figures 5.1 and 5.2 for details of standard TW sign plates.



TW - 36A

For dimensions refer to <http://www.nzta.govt.nz/resources/traffic-control-devices-manual/sign-specifications/>

Policy: A TW - 36 sign consists of an UNEVEN SURFACE symbol displayed on a standard TW sign plate.

TW - 36 signs may only be erected by employees of the controlling authority, or with the permission of the controlling authority, by contractors or other authorised persons where road surface deformation constitutes an additional hazard at a road works site.

The signs shall be removed along with all other temporary signing as soon as the road surface has been improved to a standard equal to or better than that on the approaches to the work site. This is the responsibility of the person in charge of the work party at the site.

Signs should be fitted to white painted, plain galvanised steel or plain aluminium stands. Under weather conditions likely to be experienced on all but very rare occasions the stands must be stable when:

- the centre of a TW - 36A diamond sign is located at least 1.25 m above the road surface, or
- the underside of a TW - 36B sign is located at least 1.0 m above the road surface.

Location: Normally, TW - 36 signs should be erected clear of the traffic lanes. Where this is not practicable they shall be located so as to be clearly visible to approaching drivers and not present an obstruction to traffic.

The NZTA CODE OF PRACTICE FOR TEMPORARY TRAFFIC MANAGEMENT specifies location and layout requirements for Temporary Warning signs. Special attention must be given to ensuring that the minimum clear visibility and spacing distances between the advance sign and the start of the road works site are achieved.



TW - 36B

Refer to Figures 5.1 and 5.2 for details of standard TW sign plates.

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